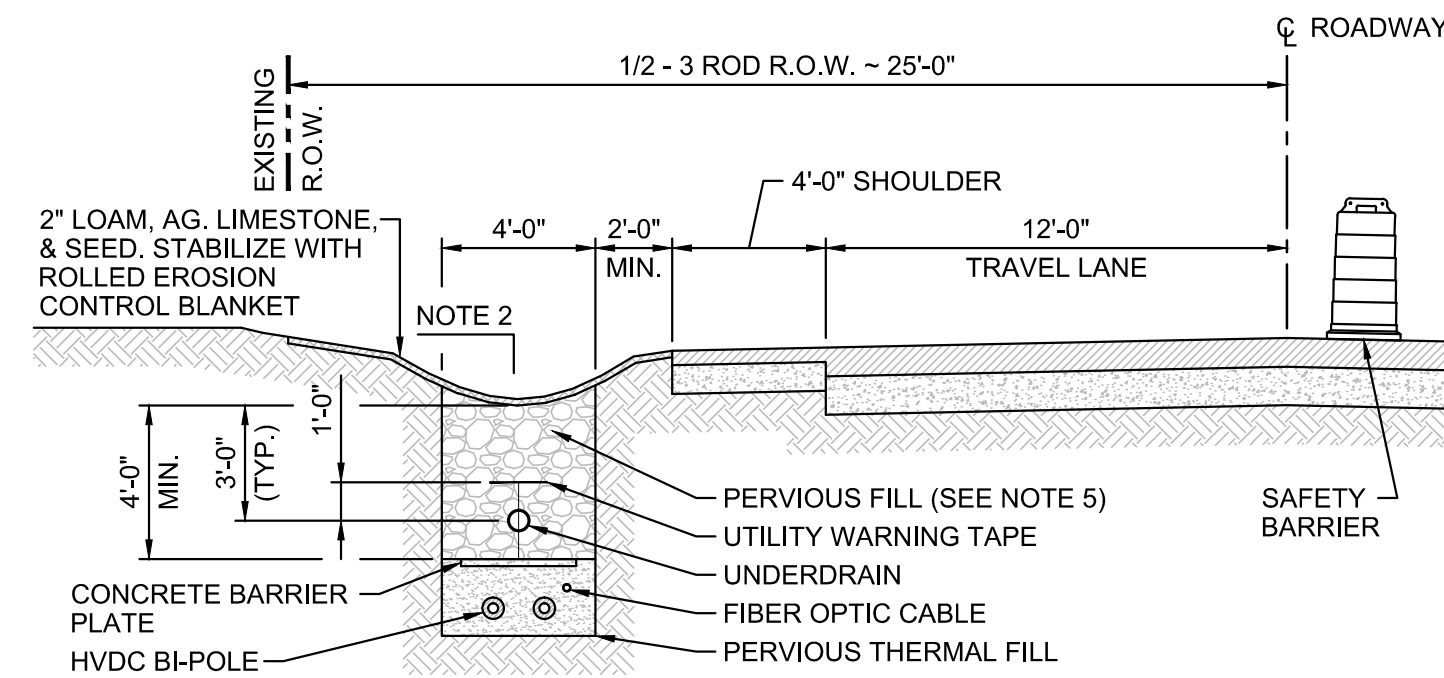


NOTES

- CABLE SPACING MAY VARY BASED UPON CONTRACTOR INSTALLATION PREFERENCE AND LOCATION. A TYPICAL SPACING OF UP TO 3 FEET IS ANTICIPATED.
- CABLES SHALL BE BEDDED IN SCREENED SAND, NATIVE SOIL OR THERMAL FILL. THERMAL FILL SHALL BE USED WHERE NATIVE MATERIAL OR SCREENED SAND DO NOT MEET MINIMUM THERMAL PROPERTIES (100°C-CM/WATT). DEPTH OF THERMAL SAND OVER CABLE SHALL BE FIELD DETERMINED FOLLOWING TESTING OF NATIVE SOILS.
- CONCRETE PROTECTIVE PLATES SHALL BE PROVIDED OVER CABLES.
- EXCAVATION MAY BE VERTICAL SHORED OR SLOPED BACK PER OSHA REQUIREMENTS WHERE NECESSARY.
- PRIOR TO EXCAVATION INSTALL EPSC MEASURES PER THE EPSC PLAN. AT THE COMPLETION OF THE WORK, CONDUCT STABILIZATION AND REMOVE EPSC MEASURES PER THE EPSC PLAN.
- ABOVE SKETCH IS TO PRESENT CONCEPTS. MORE RESTRICTIVE REQUIREMENTS OF THE RAILROAD, STATE OR OTHER AUTHORITY WILL BE REFLECTED IN THE DETAILED DESIGN.
- PRIOR TO REPLACEMENT OF TOPSOIL, THE SUBSOILS SHALL BE COMPACTED TO A DENSITY OF 95% OF THE MODIFIED PROCTOR THEORETICAL MAXIMUM DENSITY, IN ACCORDANCE WITH ASTM STANDARD D155 (STANDARD TEST METHODS FOR LABORATORY COMPACTION CHARACTERISTICS OF SOIL USING MODIFIED EFFORT).

TYPICAL TRENCH CROSS SECTION

SCALE: N.T.S.

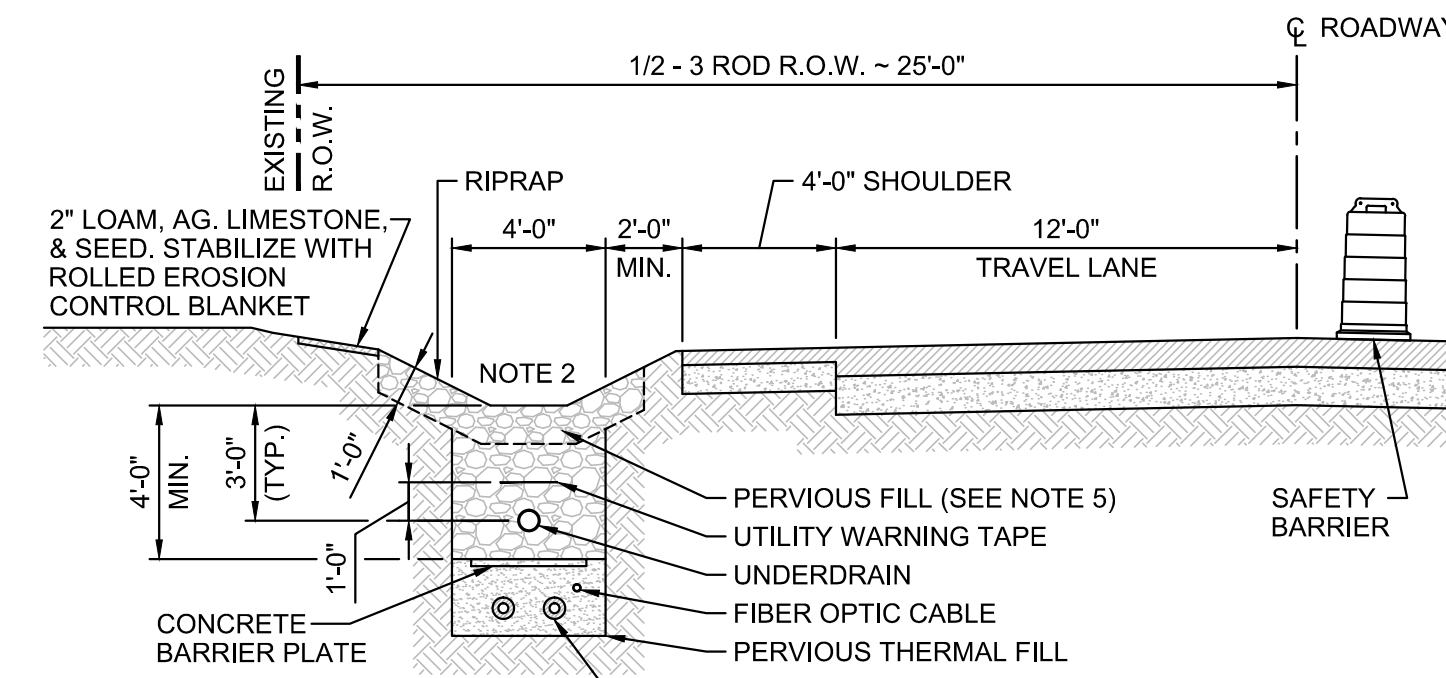


NOTES

- DRAWING DEVELOPED TO DEPICT TYPICAL INSTALLATION WITHIN DITCHLINE OF ROAD SEGMENTS UPGRADED PER THE "RECLAIM" STANDARD DESIGN.
- DITCH SIDE SLOPE VARIES. SIDE SLOPE AND GRADE TO BE RECONSTRUCTED PER VTRANS STANDARD DESIGN DETAILS UNLESS OTHERWISE DIRECTED OR APPROVED. DITCH BOTTOM SHALL BE NOT LESS THAN 2'-0" WIDE.
- EDGE OF PLANNED TRENCH EXCAVATION TO BE NOT LESS THAN 2'-0" FROM EXISTING EDGE OF PAVEMENT.
- TRENCH FILL SHALL BE PERVIOUS (P = 1X10⁻³ CM/SEC OR GREATER) WITH THERMAL RHO AS SPECIFIED.
- THERMAL FILL SHALL HAVE PERMEABILITY (P = 1X10⁻³ CM/SEC OR GREATER) AND A THERMAL RHO NOT TO EXCEED 100°C-CM/WATT UNLESS A LESSER VALUE IS SPECIFIED.
- TRENCH BACKFILL AND THERMAL FILL SHALL BE COMPACTED TO AT LEAST 95% ASTM D1557, MODIFIED PROCTOR, UNLESS OTHERWISE SPECIFIED.
- INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- TREE REMOVAL AND TRIMMING SHALL BE LIMITED TO VTRANS R.O.W. AND THE MINIMUM NECESSARY FOR PERFORMANCE OF THE WORK.
- EARTH DISTURBANCE SHALL BE LIMITED TO AREAS WITHIN LIMITS OF DISTURBANCE (LOD). DIRECT DISCHARGE TO SURFACE WATERS SHALL BE AVOIDED. DITCH OUTFALLS SHALL BE STABILIZED WITH STONE.
- DISTURBED AREAS SHALL BE RE-LOAMED, SEEDED AND AGRICULTURAL LIMESTONE APPLIED. STABILIZE DISTURBED AREAS WITH EROSION CONTROL MATTING AND OTHER MEASURES AS MAY BE REQUIRED BY THE EPSC PLAN.

ROADWAY DITCHLINE INSTALLATION

SCALE: N.T.S.

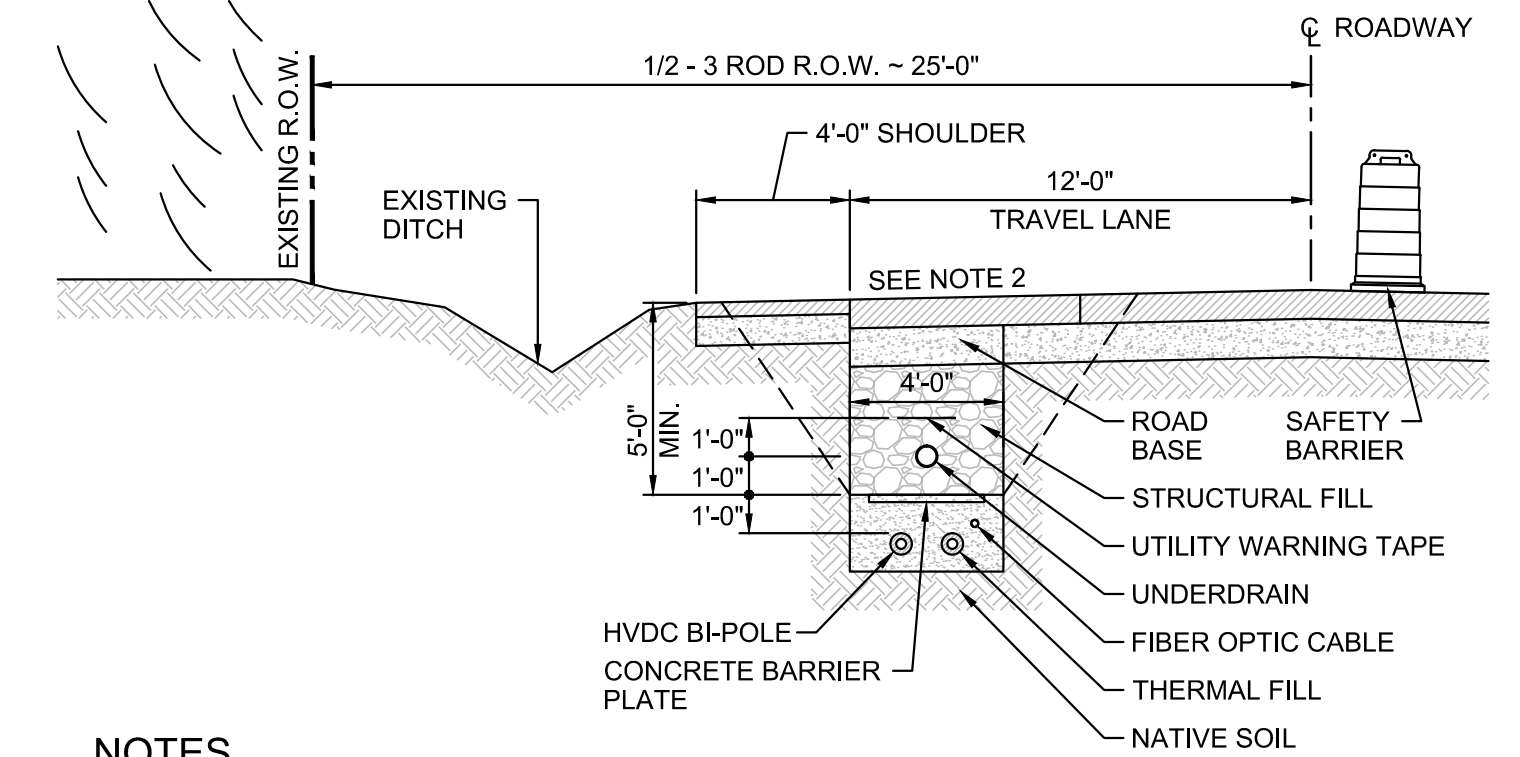


NOTES

- DRAWING DEVELOPED TO DEPICT TYPICAL INSTALLATION WITHIN DITCHLINE OF ROAD SEGMENTS UPGRADED PER THE "RECLAIM" AND "DITCH CLEANING" STANDARD DESIGNS.
- PROVIDE RIPRAP DITCH LINING FOR DITCH SECTIONS WHERE RIPRAP LINING CURRENTLY EXISTS AND OTHER LOCATIONS WHERE EROSION IS EVIDENT WITHIN THE CHANNEL. PROFILE DITCH PER VTRANS STANDARD DESIGN DETAILS WITH MINIMUM 2'-0" WIDE DITCH BOTTOM.
- EDGE OF PLANNED TRENCH EXCAVATION TO BE NOT LESS THAN 2'-0" FROM EXISTING EDGE OF PAVEMENT.
- TRENCH FILL SHALL BE PERVIOUS (P = 1X10⁻³ CM/SEC OR GREATER) WITH THERMAL RHO AS SPECIFIED.
- THERMAL FILL SHALL HAVE PERMEABILITY (P = 1X10⁻³ CM/SEC OR GREATER) AND A THERMAL RHO NOT TO EXCEED 100°C-CM/WATT UNLESS A LESSER VALUE IS SPECIFIED.
- TRENCH BACKFILL AND THERMAL FILL SHALL BE COMPACTED TO AT LEAST 95% ASTM D1557, MODIFIED PROCTOR, UNLESS OTHERWISE SPECIFIED.
- INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- TREE REMOVAL AND TRIMMING SHALL BE LIMITED TO VTRANS R.O.W. AND THE MINIMUM NECESSARY FOR PERFORMANCE OF THE WORK.
- EARTH DISTURBANCE SHALL BE LIMITED TO AREAS WITHIN LIMITS OF DISTURBANCE (LOD). DIRECT DISCHARGE TO SURFACE WATERS SHALL BE AVOIDED. DITCH OUTFALLS SHALL BE STABILIZED WITH STONE.
- DISTURBED AREAS SHALL BE RE-LOAMED, SEEDED AND AGRICULTURAL LIMESTONE APPLIED. STABILIZE DISTURBED AREAS WITH EROSION CONTROL MATTING AND OTHER MEASURES AS MAY BE REQUIRED BY THE EPSC PLAN.

ROADWAY DITCHLINE INSTALLATION W/ IMPROVED DITCH

SCALE: N.T.S.

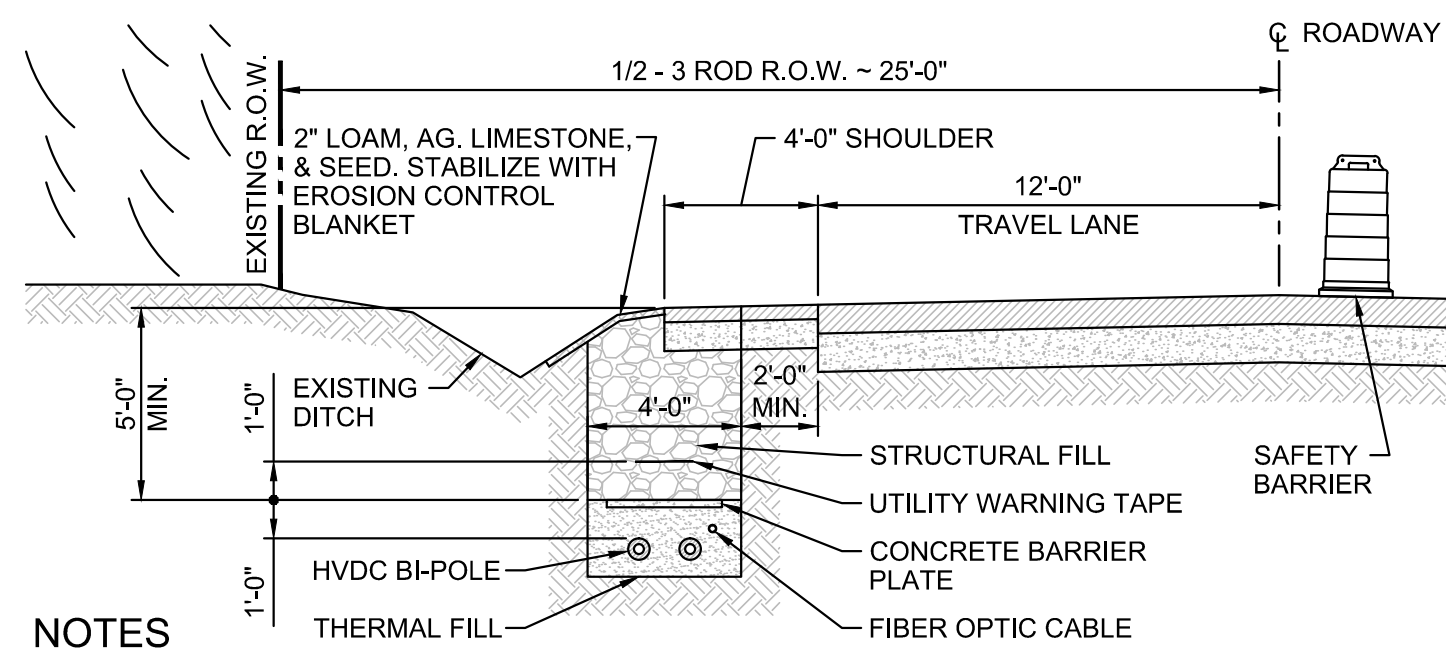


NOTES

- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET MUNICIPAL, STATE AND FEDERAL REQUIREMENTS.
- DRAWING DEVELOPED TO DEPICT TYPICAL INSTALLATION WITHIN PAVED TRAVEL LANE. LOCATE TRENCH AT EDGE OF TRAVEL LANE UNLESS NOTED OTHERWISE.
- ROADWAY PAVEMENT REPLACEMENT TO BE KEYED BY COLD MILLING A MINIMUM OF 1'-0" PER PAVEMENT COURSE. OVERALL PAVEMENT THICKNESS TO MATCH EXISTING. ALL PAVEMENT MATERIAL PER VERMONT AOT SPECIFICATIONS.
- TRENCH DEPTH 5'-0" MIN. TO TOP OF CONCRETE BARRIER. OVERALL TRENCH DEPTH VARIES BASED ON THERMAL SOIL PROPERTIES AND VTRANS REQUIREMENTS.
- STRUCTURAL FILL SHALL BE PER VERMONT AOT SPECIFICATION EXCEPT THERMAL RESISTIVITY SHALL BE 100°C-CM/WATT OR LESS UNLESS OTHERWISE SPECIFIED. NATIVE MATERIAL MAY BE USED PROVIDED IT MEETS THE SPECIFIED THERMAL RESISTIVITY.
- TRENCH SHORING IS NOT SHOWN. CONTRACTOR SHALL SHORE OR BENCH EXCAVATION TO MEET FEDERAL AND STATE SAFETY REQUIREMENTS.
- INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- EARTH DISTURBANCE SHALL BE LIMITED TO AREAS WITHIN LIMITS OF DISTURBANCE (LOD). DIRECT DISCHARGE TO SURFACE WATERS SHALL BE AVOIDED. DITCH OUTFALLS SHALL BE STABILIZED WITH STONE.
- DISTURBED AREAS SHALL BE RE-LOAMED, SEEDED AND AGRICULTURAL LIMESTONE APPLIED. STABILIZE DISTURBED AREAS WITH EROSION CONTROL MATTING AND OTHER MEASURES AS MAY BE REQUIRED BY THE EPSC PLAN.

ROADWAY TRENCH SECTION IN PAVEMENT

SCALE: N.T.S.

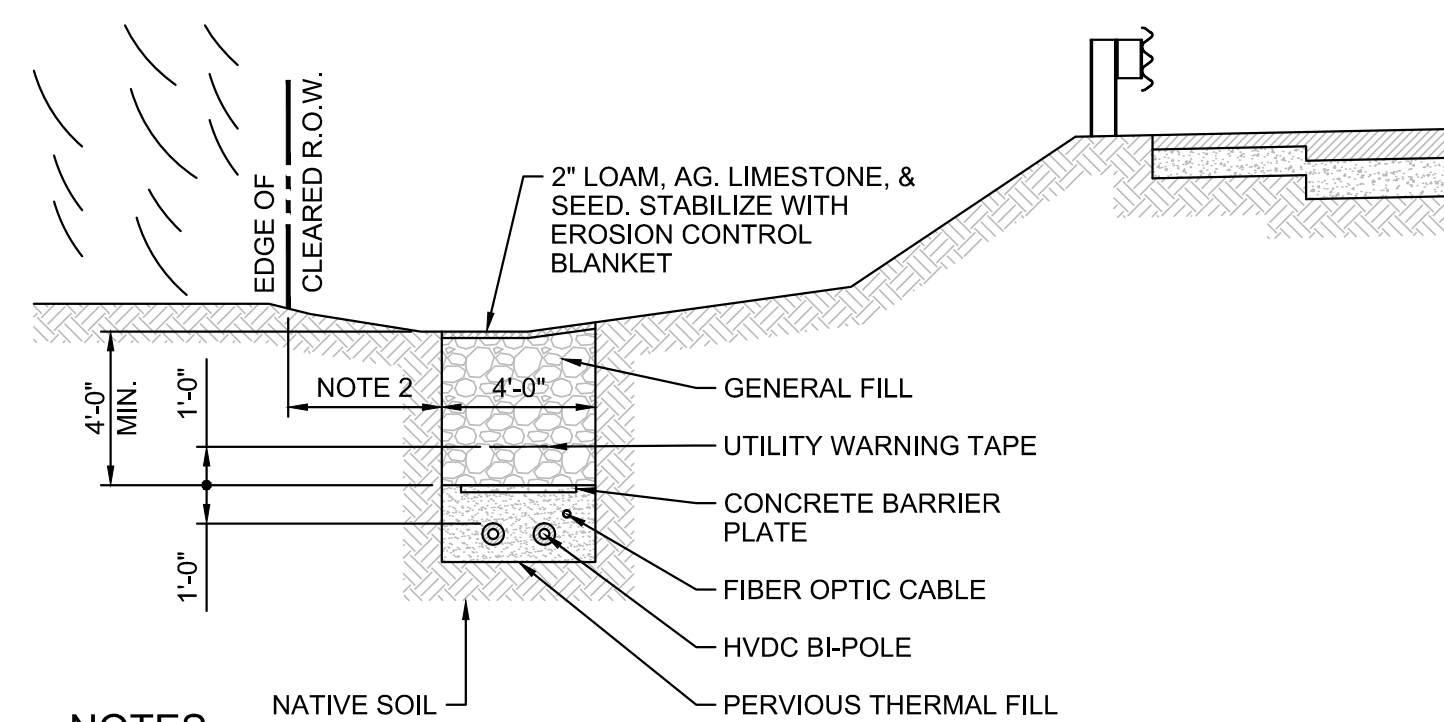


NOTES

- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET MUNICIPAL, STATE AND FEDERAL REQUIREMENTS.
- DRAWING DEVELOPED TO DEPICT TYPICAL INSTALLATION WITHIN ROAD SHOULDER/BREAKDOWN LANE.
- ROADWAY SHOULDER/BREAKDOWN LANE PAVEMENT REPLACEMENT TO BE KEYED BY COLD MILLING OF A MINIMUM OF 1'-0" PER PAVEMENT COURSE. OVERALL PAVEMENT THICKNESS TO MATCH EXISTING. ALL PAVEMENT MATERIAL PER VERMONT AOT SPECIFICATIONS.
- EDGE OF PLANNED TRENCH EXCAVATION TO BE NOT LESS THAN 2'-0" FROM EXISTING EDGE OF TRAVEL LANE PAVEMENT.
- TRENCH DEPTH 5'-0" MIN. TO TOP OF CONCRETE BARRIER. OVERALL TRENCH DEPTH VARIES BASED ON THERMAL SOIL PROPERTIES AND VTRANS REQUIREMENTS.
- STRUCTURAL FILL SHALL BE PER VERMONT AOT SPECIFICATION EXCEPT THERMAL RESISTIVITY SHALL BE 100°C-CM/WATT OR LESS UNLESS OTHERWISE SPECIFIED. NATIVE MATERIAL MAY BE USED PROVIDED IT MEETS THE SPECIFIED THERMAL RESISTIVITY.
- TRENCH SHORING IS NOT SHOWN. CONTRACTOR SHALL SHORE OR BENCH EXCAVATION TO MEET FEDERAL AND STATE SAFETY REQUIREMENTS.
- INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- TREE REMOVAL AND TRIMMING SHALL BE LIMITED TO VTRANS R.O.W. AND THE MINIMUM NECESSARY FOR PERFORMANCE OF THE WORK.
- EARTH DISTURBANCE SHALL BE LIMITED TO AREAS WITHIN LIMITS OF DISTURBANCE (LOD). DIRECT DISCHARGE TO SURFACE WATERS SHALL BE AVOIDED. DITCH OUTFALLS SHALL BE STABILIZED WITH STONE.
- DISTURBED AREAS SHALL BE RE-LOAMED, SEEDED AND AGRICULTURAL LIMESTONE APPLIED. STABILIZE DISTURBED AREAS WITH EROSION CONTROL MATTING AND OTHER MEASURES AS MAY BE REQUIRED BY THE EPSC PLAN.

ROADWAY TRENCH SECTION IN SHOULDER

SCALE: N.T.S.

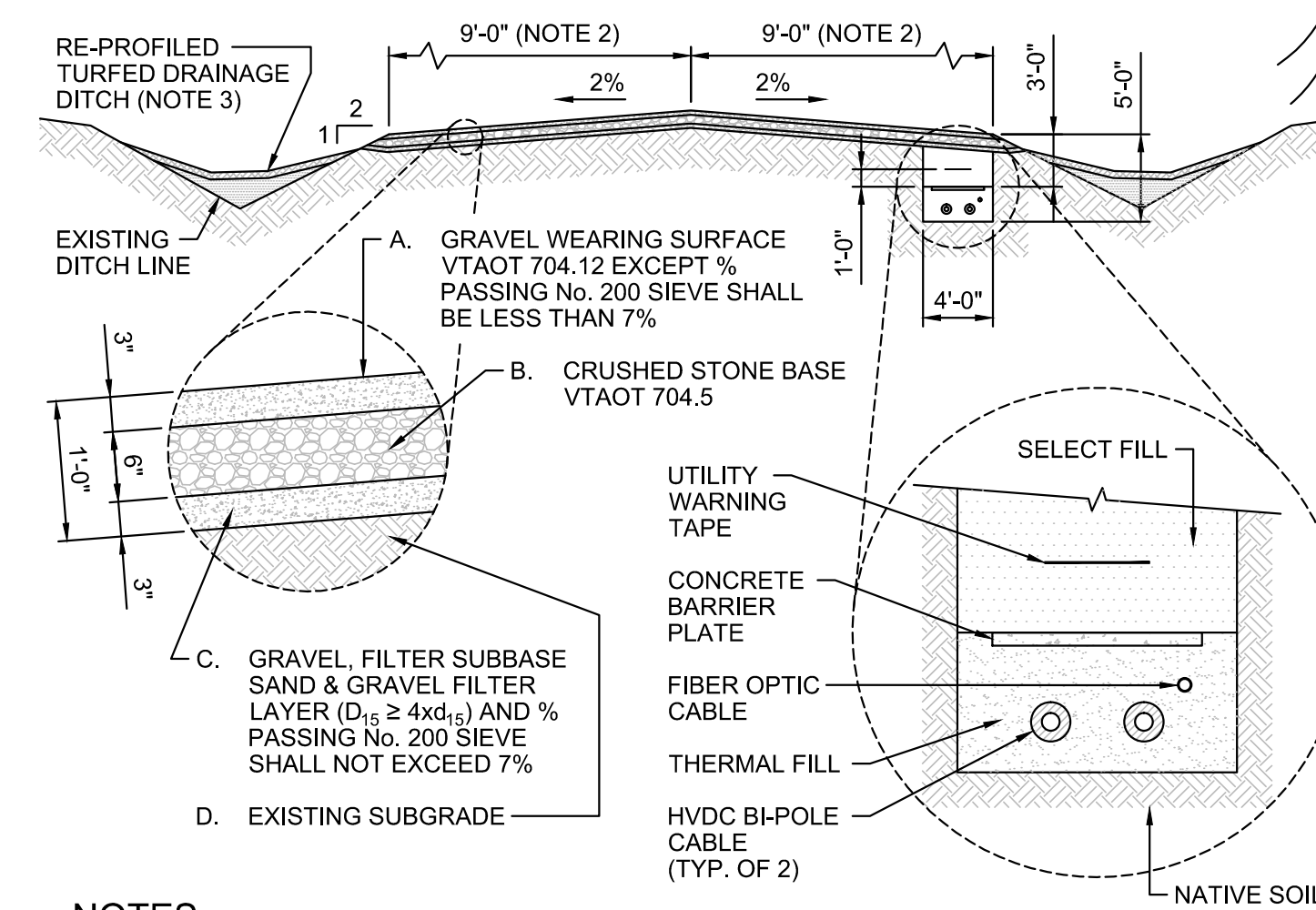


NOTES

- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET MUNICIPAL, STATE AND FEDERAL REQUIREMENTS.
- CABLE TO BE LOCATED AT EDGE OF CLEARED R.O.W. WITH SUFFICIENT SPACE TO ERECT REQUIRED SAFETY AND ENVIRONMENTAL CONTROLS UNLESS OTHERWISE APPROVED BY VTRANS.
- PROVIDE THERMAL FILL AS REQUIRED.
- TRENCH GENERAL BACKFILL SHALL BE NATIVE SOIL COMPACTED TO MATCH IN-SITU SOIL DENSITY UNLESS OTHERWISE SPECIFIED. NATIVE SOIL EXCEEDING THERMAL RESISTIVITY OF 100°C-CM/WATT SHALL BE REPLACED WITH MORE SUITABLE MATERIAL.
- TRENCH SHORING IS NOT SHOWN. CONTRACTOR SHALL SHORE OR BENCH EXCAVATION TO MEET FEDERAL AND STATE SAFETY REQUIREMENTS.
- INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
- TREE REMOVAL AND TRIMMING SHALL BE LIMITED TO VTRANS R.O.W. AND THE MINIMUM NECESSARY FOR PERFORMANCE OF THE WORK.
- EARTH DISTURBANCE SHALL BE LIMITED TO AREAS WITHIN LIMITS OF DISTURBANCE (LOD). DIRECT DISCHARGE TO SURFACE WATERS SHALL BE AVOIDED. DITCH OUTFALLS SHALL BE STABILIZED WITH STONE.
- DISTURBED AREAS SHALL BE RE-LOAMED, SEEDED AND AGRICULTURAL LIMESTONE APPLIED. STABILIZE DISTURBED AREAS WITH EROSION CONTROL MATTING AND OTHER MEASURES AS MAY BE REQUIRED BY THE EPSC PLAN.
- AT COMPLETION OF THE WORK, RESTORE CONSTRUCTION SITE TO MATCH SURROUNDING TURFED SURFACES.

ROADWAY TRENCH SECTION IN TURFED AREA

SCALE: N.T.S.



NOTES

- DRAWING DEVELOPED TO DEPICT PROPOSED GRAVEL ROAD IMPROVEMENTS ALONG PROJECT ROUTE IN BENSON. ROADS INCLUDE NORTH LAKE ROAD, STONY POINT ROAD AND OLD NORTH LAKE ROAD.
- ROAD GRAVEL TO BE APPLIED OVER THE LESSER OF THE ENTIRE LANE WIDTH INDICATED OR TO EDGE OF EXISTING ROADWAY.
- PROVIDE ROAD DITCH CLEANING AND PROFILING WHERE GRADES PERMIT SUCH ALTERATIONS.
- EXISTING ROAD SURFACE SHALL BE GRADED TO PROVIDE UNIFORM CROSS-SLOPE TO MATCH FINISHED ROAD CROWN. COMPACT TOP SIX INCHES OF SUBGRADE TO NOT LESS THAN 95% ASTM 1557 PRIOR TO SUBBASE APPLICATION.
- SUBBASE, BASE, WEARING SURFACE AND TRENCH BACKFILL SHALL BE COMPACTED TO 95% ASTM 1557, THEORETICAL MAXIMUM DENSITY.
- EXISTING DRIVEWAY APRON SHALL BE ADJUSTED TO MATCH RE-BUILT ROAD ELEVATION.

TYPICAL BENSON ROADWAY SECTION

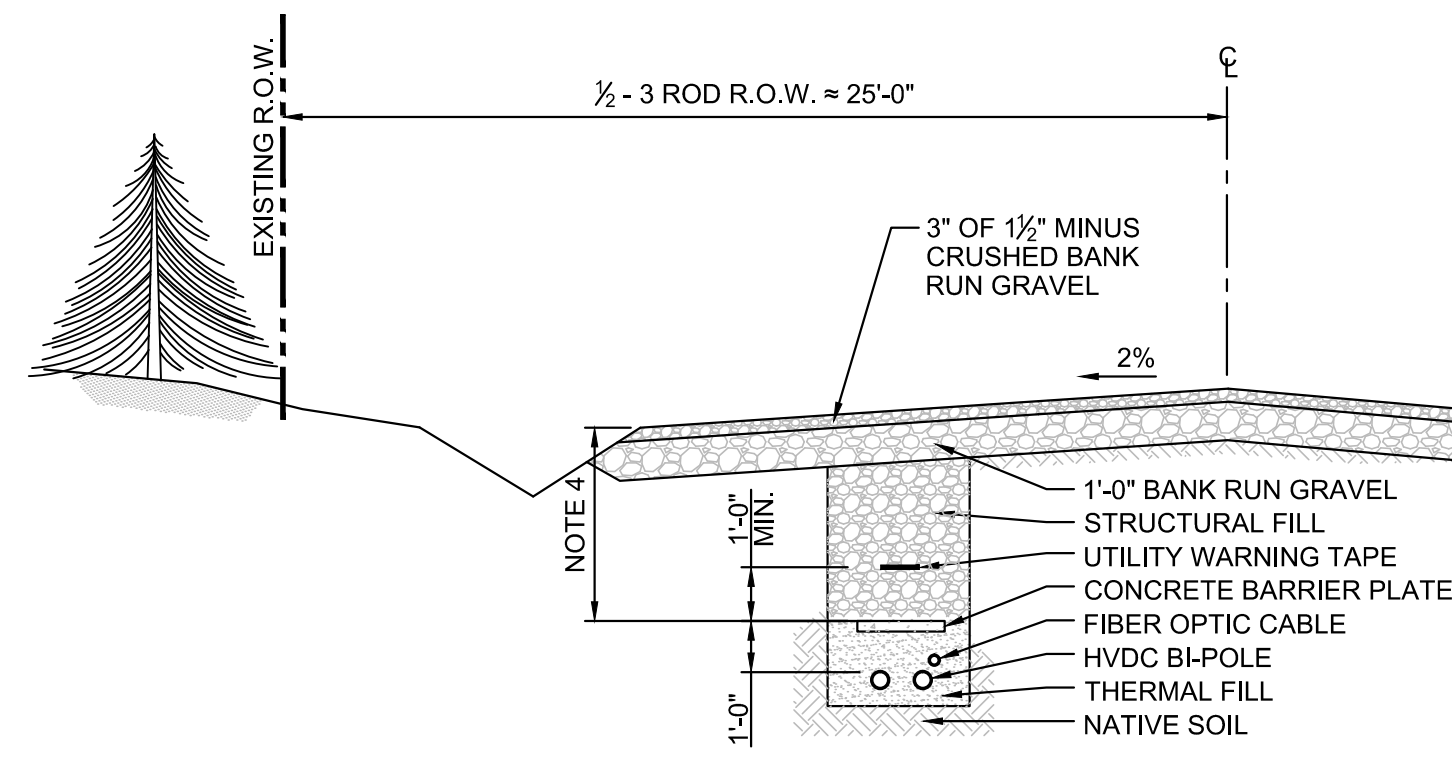
SCALE: N.T.S.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

No.	Revision	Date	By	Ck	PE	PE #
A	20% ANR Submission	12/5/14	TRC	AMW		
B	EPSC & PERMITS FCR	3/6/15	TRC	AMW		
C	ISSUED FOR USE	3/27/15	TRC	AMW		
D	ISSUED FOR PERMITTING	7/24/15	TRC	AMW		

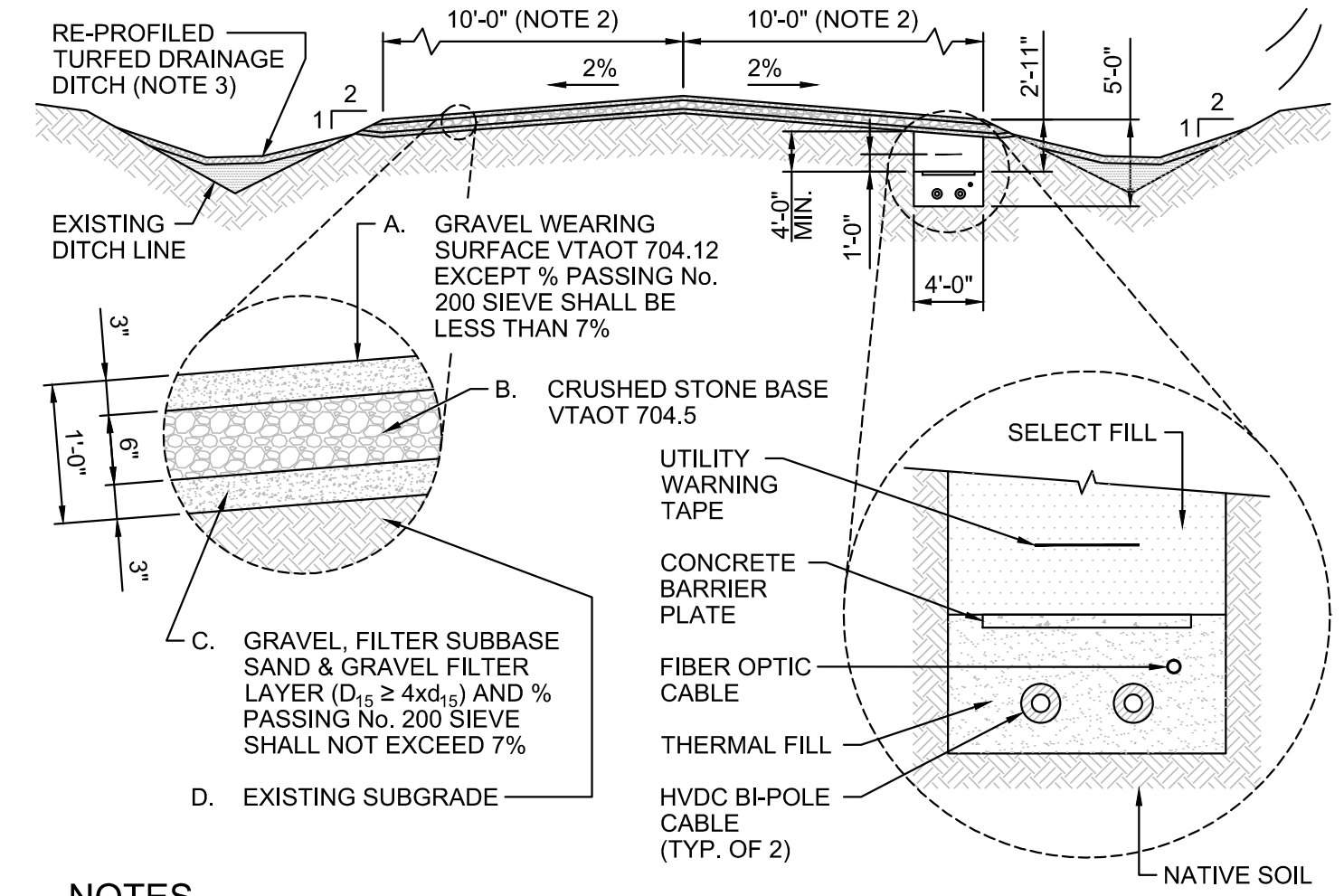
TDI New England

New England Clean Power Link
 TDI New England
 Typical Details



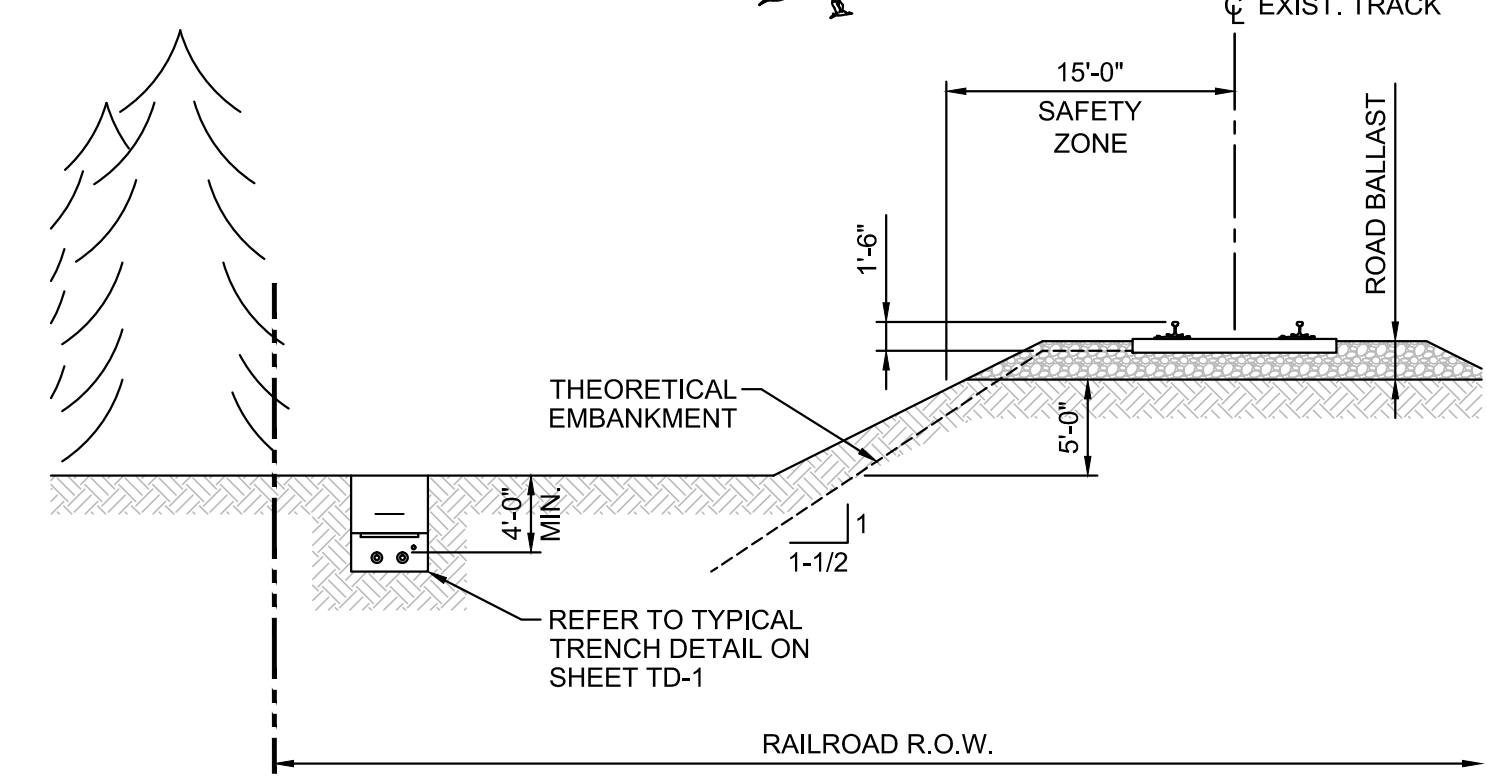
- NOTES**
- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET TOWN OF LUDLOW, STATE AND FEDERAL REQUIREMENTS.
 - ROADWAY WIDTH VARIES.
 - ROADWAY GRAVEL SHALL MEET THE MINIMUM STANDARDS OF THE TOWN OF LUDLOW, VERMONT TOWN ROAD & BRIDGE STANDARDS.
 - TRENCH DEPTH 3'-0" TO CONCRETE BARRIER, 4'-0" TO TOP OF DC CABLE. OVERALL TRENCH DEPTH VARIES BASED ON THERMAL SOIL PROPERTIES.
 - STRUCTURAL FILL SHALL BE PER VERMONT AOT SPECIFICATION EXCEPT THERMAL RESISTIVITY SHALL BE 100°C-CM/WATT OR LESS UNLESS OTHERWISE SPECIFIED.
 - TRENCH BACKFILL SHALL BE THERMAL FILL AS REQUIRED TO MEET CALCULATED THERMAL CONDUCTIVITY REQUIREMENTS OF THE DESIGN.
 - UTILITY WARNING TAPE SHALL BE PLACED NOT LESS THAN 1'-0" ABOVE THE CONCRETE BARRIER PLATE.

LUDLOW ROADWAY SECTION
SCALE: N.T.S.



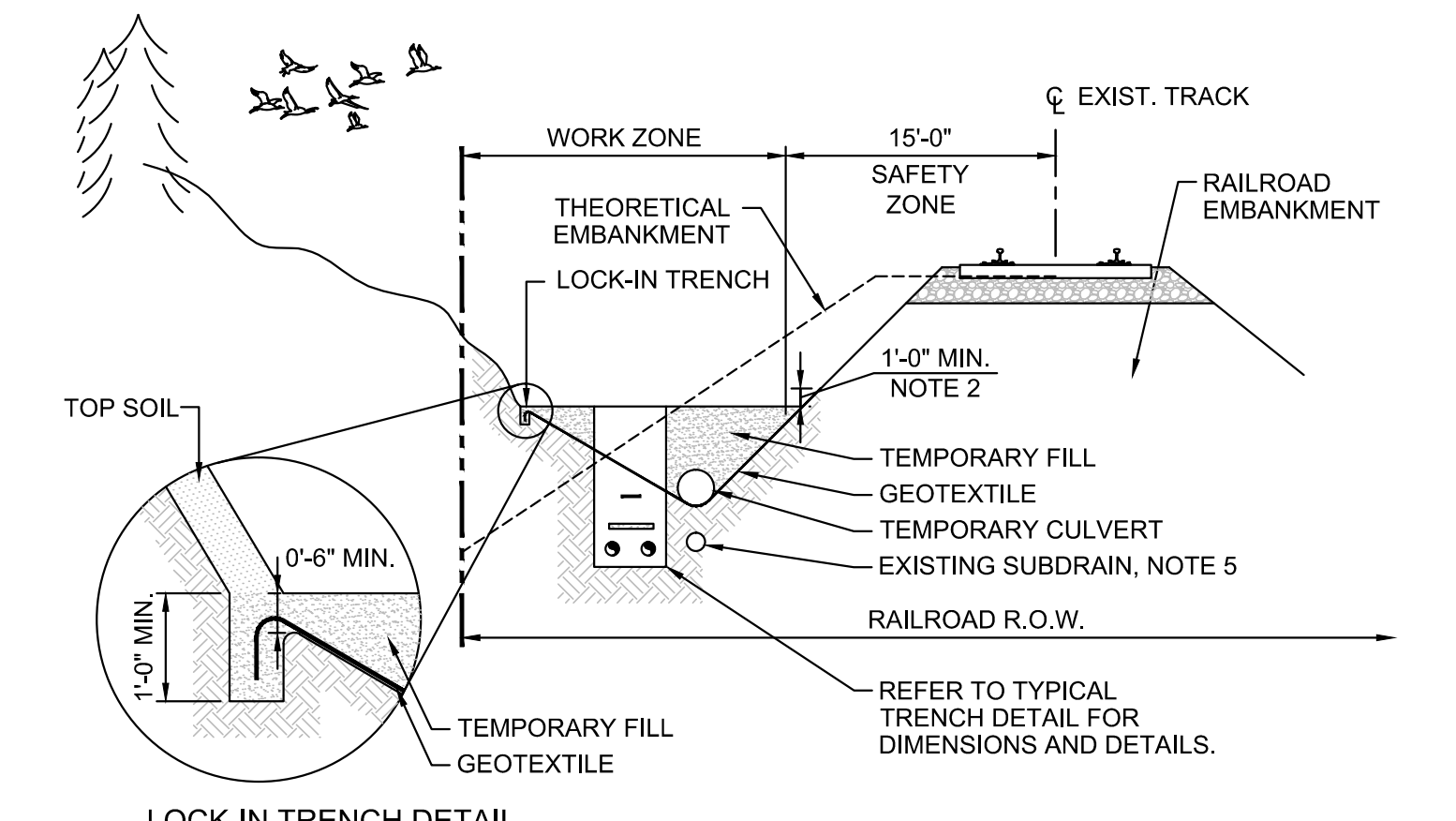
- NOTES**
- DRAWING DEVELOPED TO DEPICT PROPOSED GRAVEL ROAD IMPROVEMENTS ALONG PROJECT ROUTE IN ALBURGH.
 - ROAD GRAVEL TO BE APPLIED OVER THE LESSER OF THE ENTIRE LANE WIDTH INDICATED OR TO EDGE OF EXISTING ROADWAY.
 - PROVIDE ROAD DITCH CLEANING AND PROFILING WHERE GRADES PERMIT SUCH ALTERATIONS.
 - EXISTING ROAD SURFACE SHALL BE GRADED TO PROVIDE UNIFORM CROSS-SLOPE TO MATCH FINISHED ROAD CROWN. COMPACT TOP SIX INCHES OF SUBGRADE TO NOT LESS THAN 95% ASTM 1557 PRIOR TO SUBBASE APPLICATION.
 - SUBBASE, BASE, WEARING SURFACE AND TRENCH BACKFILL SHALL BE COMPACTED TO 95% ASTM 1557, THEORETICAL MAXIMUM DENSITY.
 - EXISTING DRIVEWAY APRON SHALL BE ADJUSTED TO MATCH RE-BUILT ROAD ELEVATION.

ALBURGH ROADWAY SECTION
SCALE: N.T.S.



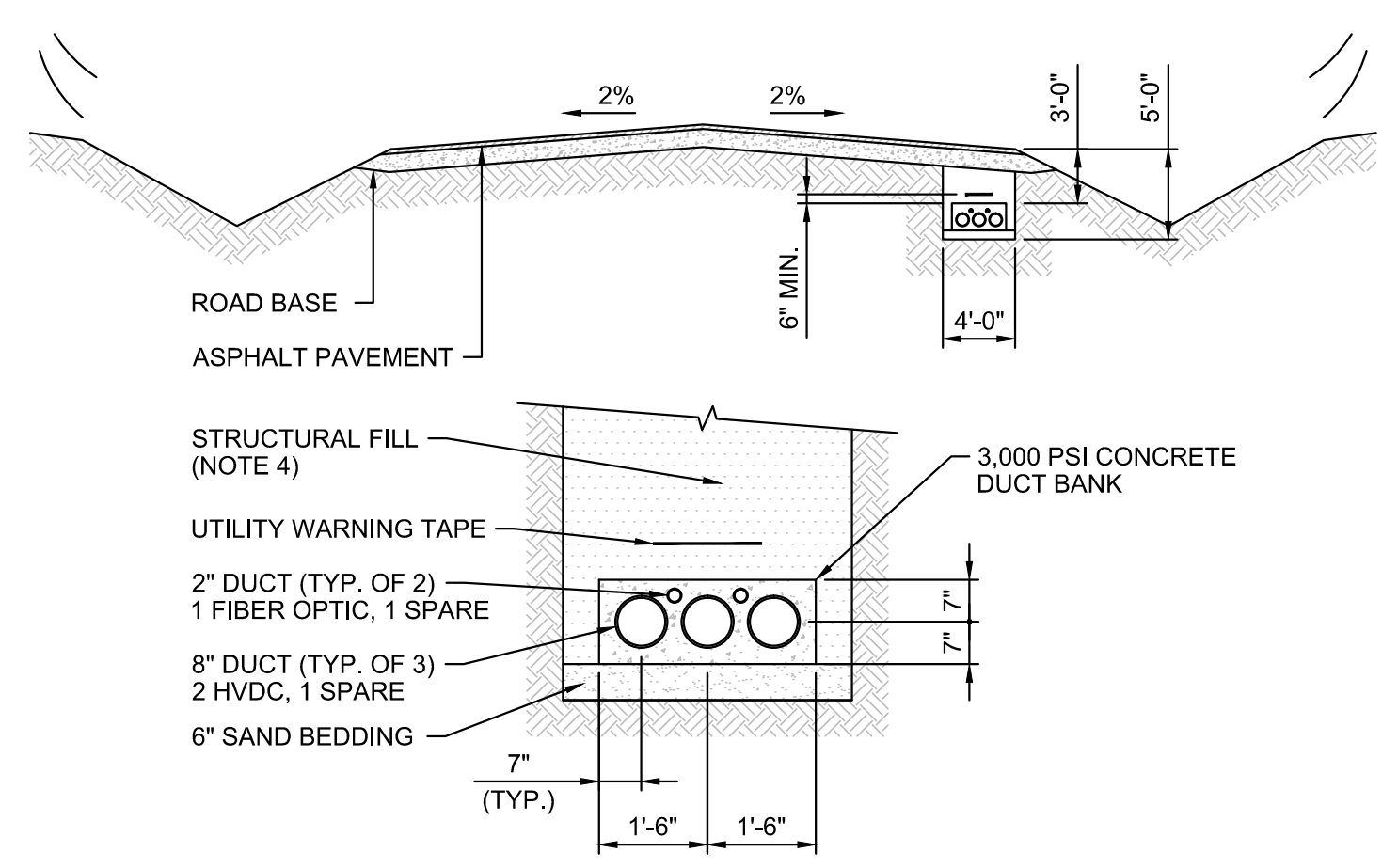
- NOTES**
- THE THEORETICAL EMBANKMENT IS THE THEORETICAL LIMIT OF THE RAILROAD FOUNDATION CARRYING THE DYNAMIC TRACK LOADING. THE EMBANKMENT STARTS AT A DEPTH OF 1-1/2 FT. BELOW THE TOP OF THE RAILS AND 10 FT. FROM THE TRACK CENTERLINE, EXTENDING AT A SLOPE OF 1-1/2H:1V TO A DEPTH OF AT LEAST 5 FT. BELOW THE ROAD BALLAST.
 - ANY EXCAVATION PENETRATING THE THEORETICAL EMBANKMENT SHALL BE SHORED. SHORING SHALL BE DEVELOPED TO CARRY E-30 LOADING FOLLOWING PROCEDURES ESTABLISHED BY AREMA CHAPTER 8, SECTION 20 AND 28.
 - DURING TRAIN MOVEMENT, ALL PERSONNEL AND EQUIPMENT SHALL BE OUTSIDE THE SAFETY ZONE, AND ALL EQUIPMENT WITHIN 50 FT. OF THE TRACK SHALL BE SHUT DOWN AND OPERATORS OUT OF THE VEHICLES.
 - THE TYPICAL SECTION AND REQUIREMENTS ARE PRESENTED FOR CONCEPT ONLY. ADDITIONAL AND MORE STRINGENT REQUIREMENTS MAY BE REQUIRED BY THE OPERATING RAILROAD, FEDERAL, STATE AND LOCAL REGULATIONS.
 - PROVIDE EROSION AND SEDIMENT CONTROLS AS REQUIRED BY APPROVED PERMITS, VT. STANDARDS & SPECIFICATIONS FOR EROSION PREVENTION & SEDIMENT CONTROL, AND AS DIRECTED.

TYPICAL RAILROAD SECTION



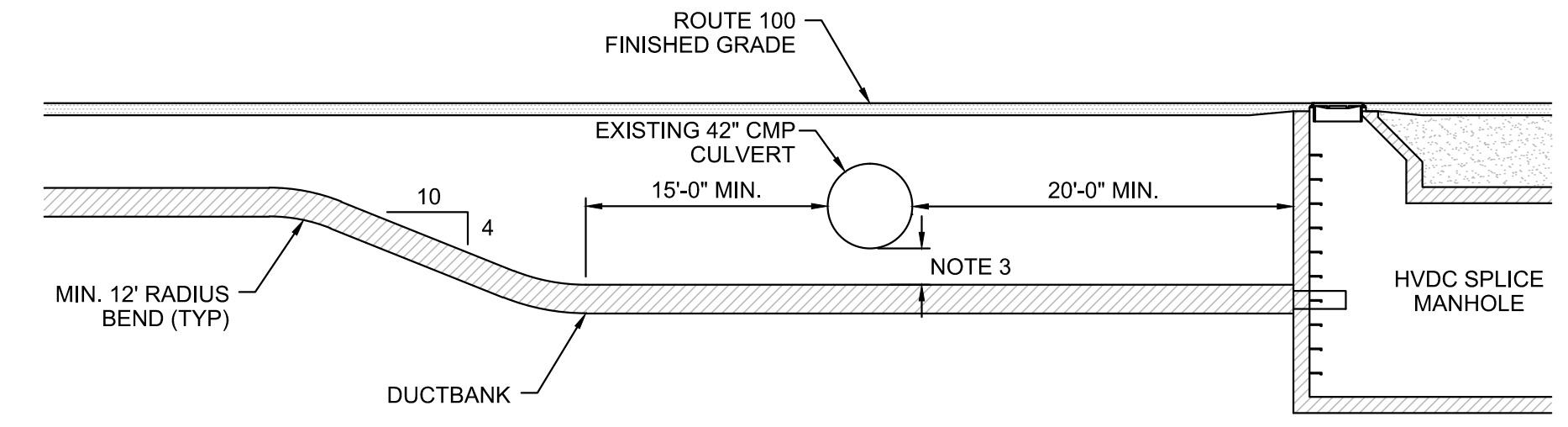
- NOTES**
- AFTER CLEARING WORK ZONE, AREAS TO RECEIVE TEMPORARY FILL SHALL BE STRIPPED OF TOPSOIL PRIOR TO PLACING GEOTEXTILE OVER THE EXPOSED SUBGRADE.
 - EDGE OF GEOTEXTILE SHALL BE ANCHORED IN A LOCK-IN TRENCH ON THE OUTER EDGE OF THE R.O.W. AND SURFACE LAID AGAINST THE RAILROAD EMBANKMENT.
 - WIDTH OF FILL AREA VARIES WITH SITE TOPOGRAPHY.
 - PROVIDE TEMPORARY PERFORATED CULVERT TO COLLECT AND DIRECT GROUNDWATER TO ESTABLISHED DRAINAGE STRUCTURES.
 - CONTRACTOR SHALL BE AWARE DRAINAGE DITCHES ALONG THE RAILROAD MAY BE UNDERLAIN WITH EXISTING PERFORATED PIPE SUBDRAINS. CABLE TRENCH SHALL EITHER AVOID THE EXISTING OR REMOVE AND REPLACE THEM AS THE WORK PROGRESSES.
 - EXISTING SUBGRADE SHALL BE PROTECTED BY A WOVEN GEOTEXTILE. THE GEOTEXTILE IS INTENDED TO PROVIDE STABILITY AND SEPARATION OF THE EXISTING SOIL AND TEMPORARY FILL.
 - TEMPORARY FILL SHALL BE GRANULAR, FREE DRAINING BANK RUN GRAVEL, CRUSHED GRAVEL, OR SAND.
 - UPON COMPLETION OF THE WORK, REMOVE THE TEMPORARY FILL, CULVERT, AND GOETEXTILE. RESTORE THE SITE TO ITS ORIGINAL CONDITION.

RAILROAD ADJACENT TEMPORARY FILL DETAIL



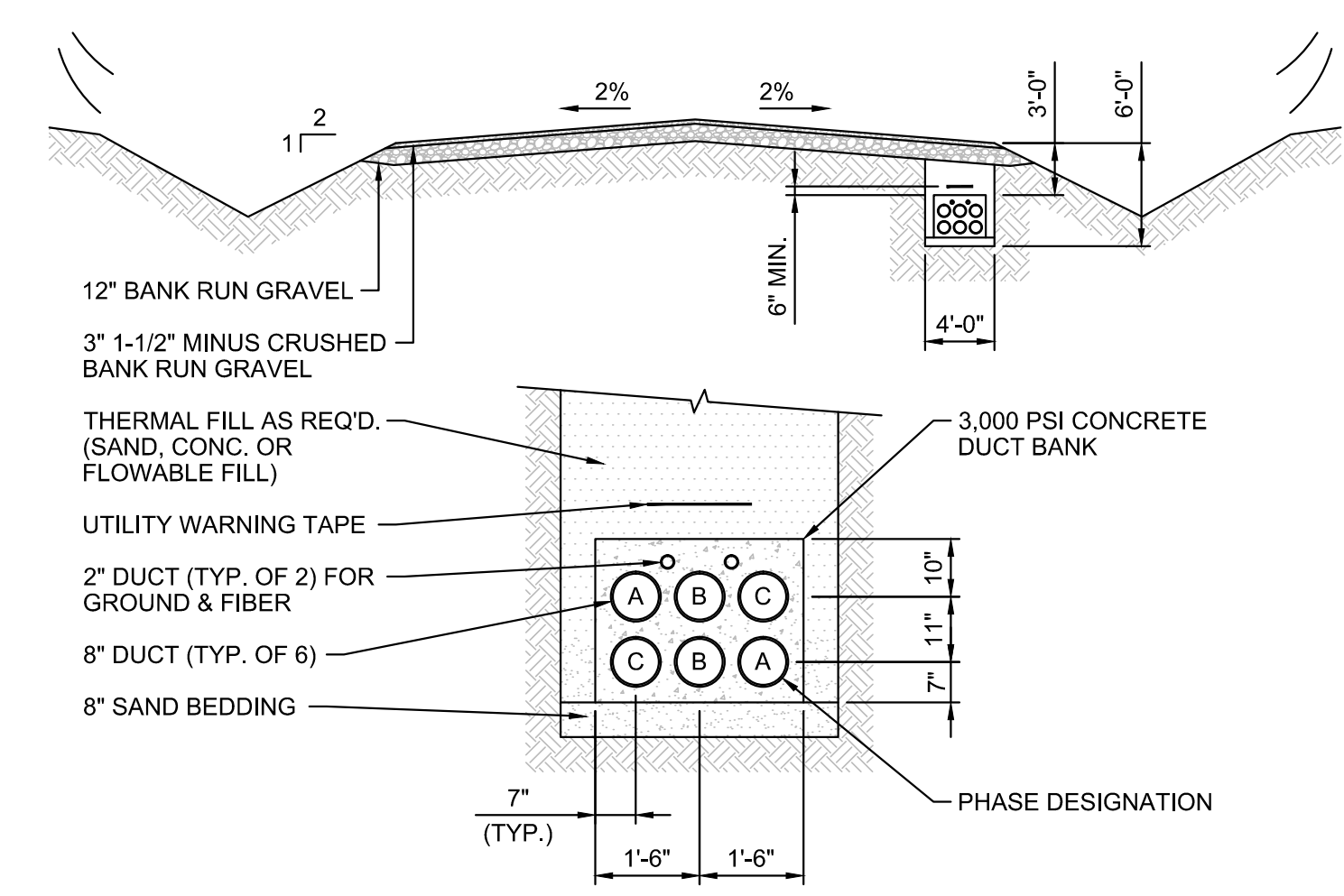
- NOTES**
- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET STATE AND FEDERAL REQUIREMENTS.
 - UTILITY WARNING TAPE SHALL BE PLACED NOT LESS THAN 6 INCHES ABOVE THE TOP OF THE DUCT BANK CONCRETE.
 - DUCTBANK BURIAL DEPTH VARIES ALONG THE ROUTE. MINIMUM BURIAL DEPTH TO TOP OF DUCTBANK CONCRETE SHALL BE 3 FEET.
 - STRUCTURAL FILL SHALL BE PER VERMONT AOT SPECIFICATIONS EXCEPT THERMAL RESISTIVITY SHALL BE 100°C-CM/WATT OR LESS. EXISTING ROAD MATERIAL MAY BE USED PROVIDED IT MEETS THE SPECIFIED THERMAL RESISTIVITY.

STATE ROUTE 100 HVDC DUCTBANK SECTION
SCALE: N.T.S.



- NOTES**
- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION FOR CONFORMANCE WITH STATE AND FEDERAL REQUIREMENTS.
 - DUCTBANK ELEVATION TRANSITION SHALL SLOPE AT APPROXIMATELY 10H:2V UNLESS OTHERWISE APPROVED.
 - DUCTBANK SHALL PASS NOT LESS THAN 5 FEET BELOW THE EXISTING CULVERT. THE 5 FOOT DEPTH IS REQUIRED TO ACCOMMODATE FUTURE CULVERT UPGRADES.
 - LOCATE HVDC SPLICE MANHOLE APPROXIMATELY AS INDICATED BUT NOT LESS THAN 20 FEET FROM THE EXISTING CULVERT.
 - DUCTBANK BETWEEN CULVERT AND MANHOLE SHALL BE STRAIGHT WITH NO CHANGES OF ELEVATION OR DIRECTION.
 - DUCTBANK SHALL BE INSTALLED WITHOUT MODIFICATION OF THE EXISTING CULVERT. PROVIDE CULVERT BRACING AND OTHER TEMPORARY SUPPORTS AS NECESSARY.

STATE ROUTE 100 HVDC DUCTBANK STREAM CROSSING
SCALE: N.T.S.



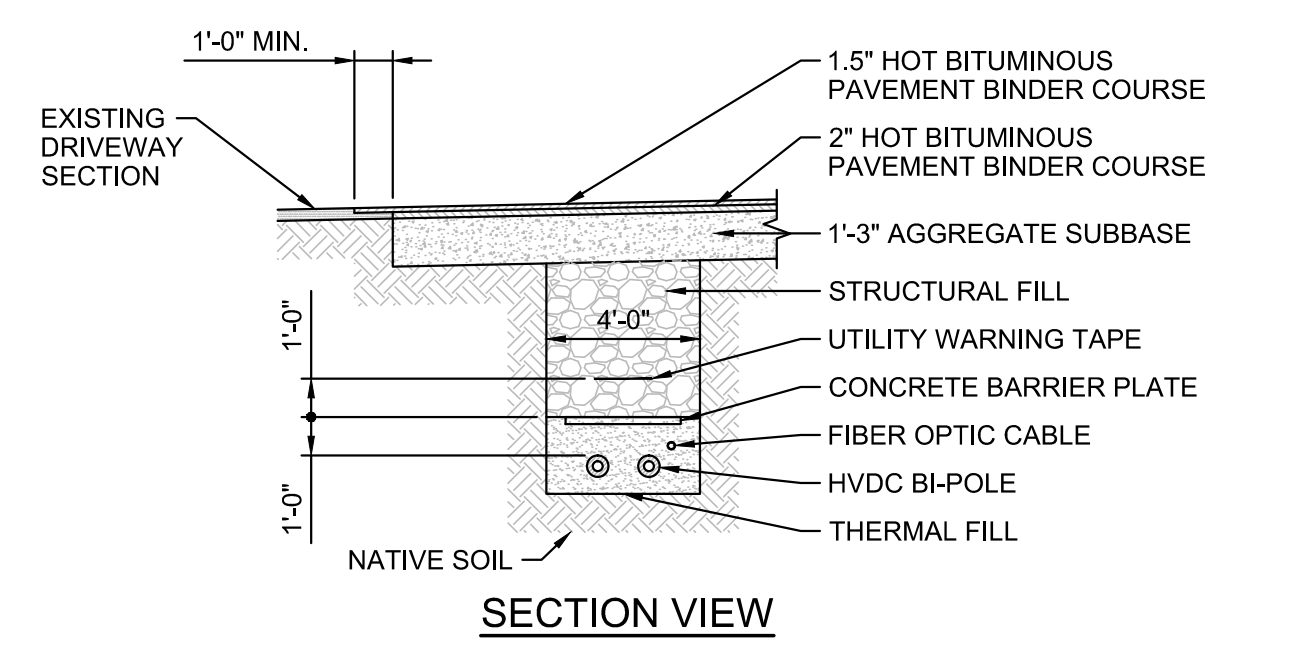
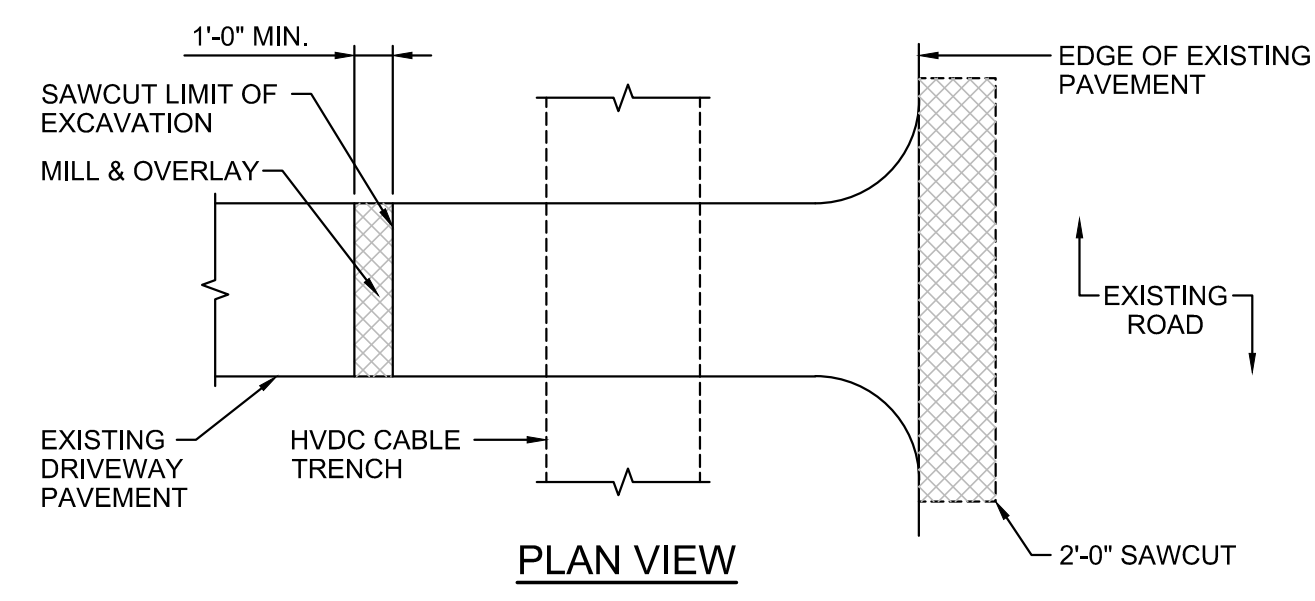
- NOTES**
- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET TOWN OF LUDLOW, STATE AND FEDERAL REQUIREMENTS.
 - ROADWAY WIDTH VARIES.
 - ROADWAY GRAVEL SHALL MEET THE MINIMUM STANDARDS OF THE TOWN OF LUDLOW, VERMONT TOWN ROAD & BRIDGE STANDARDS.
 - TRENCH BACKFILL SHALL BE THERMAL FILL AS REQUIRED TO MEET CALCULATED THERMAL CONDUCTIVITY REQUIREMENTS OF THE DESIGN.
 - UTILITY WARNING TAPE SHALL BE PLACED NOT LESS THAN 6 INCHES ABOVE THE DUCT BANK CONCRETE.

MUNICIPAL ROADWAY HVAC SECTION
SCALE: N.T.S.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

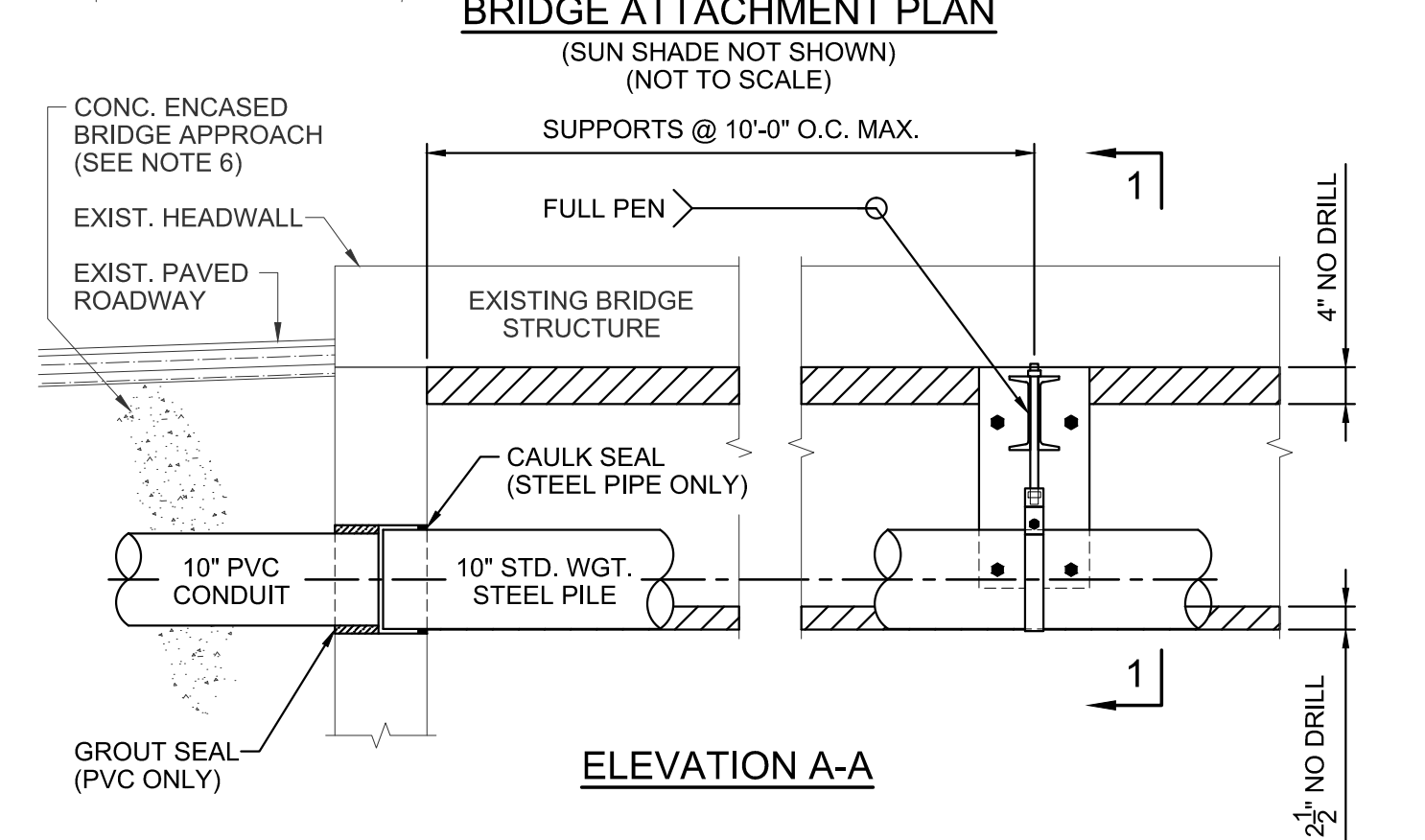
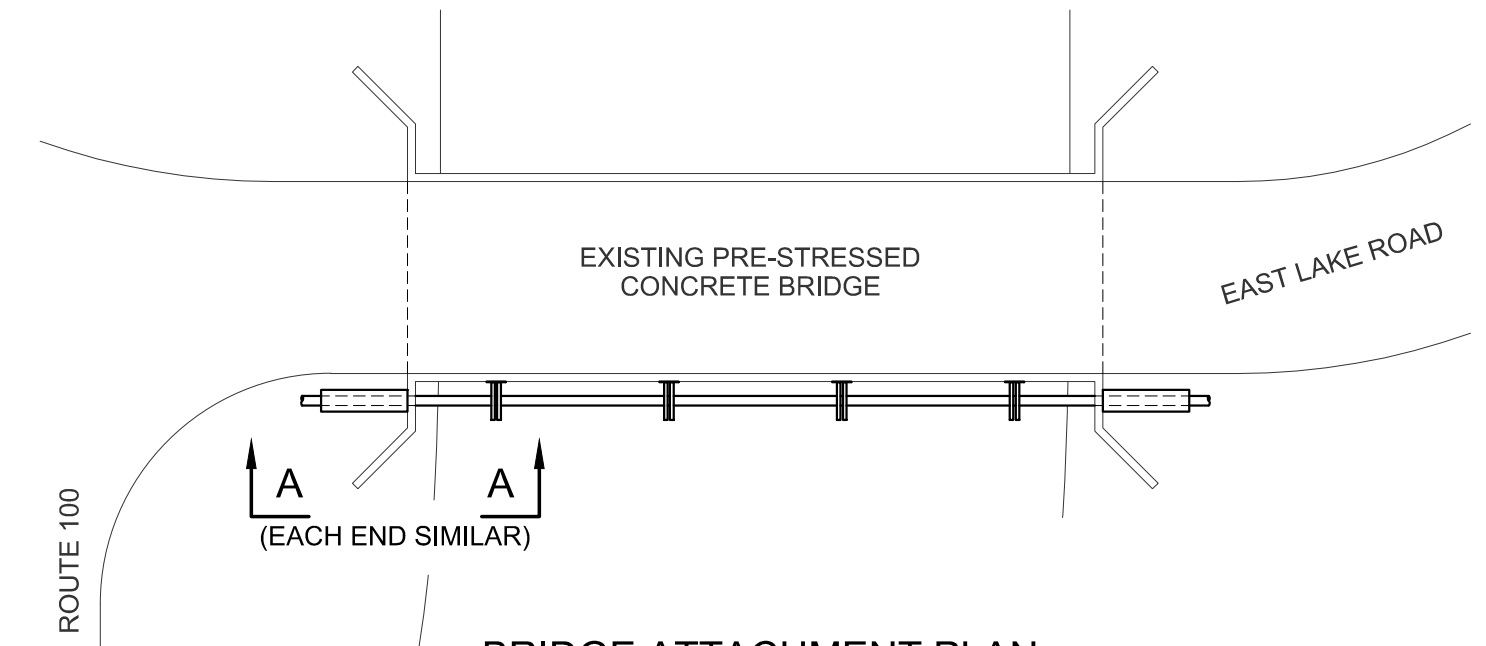
No.	Revision	Date	By	Ck	PE	PE #
A	20% ANR Submission	12/5/14	TRC	AMW		
B	EPSC & PERMITS FCR	3/6/15	TRC	AMW		
C	ISSUED FOR USE	3/27/15	TRC	AMW		
D	ISSUED FOR PERMITTING	7/24/15	TRC	AMW		

TDI New England
New England Clean Power Link
TDI New England
Typical Details
TD-2
Prepared by: TRC 10/09/14



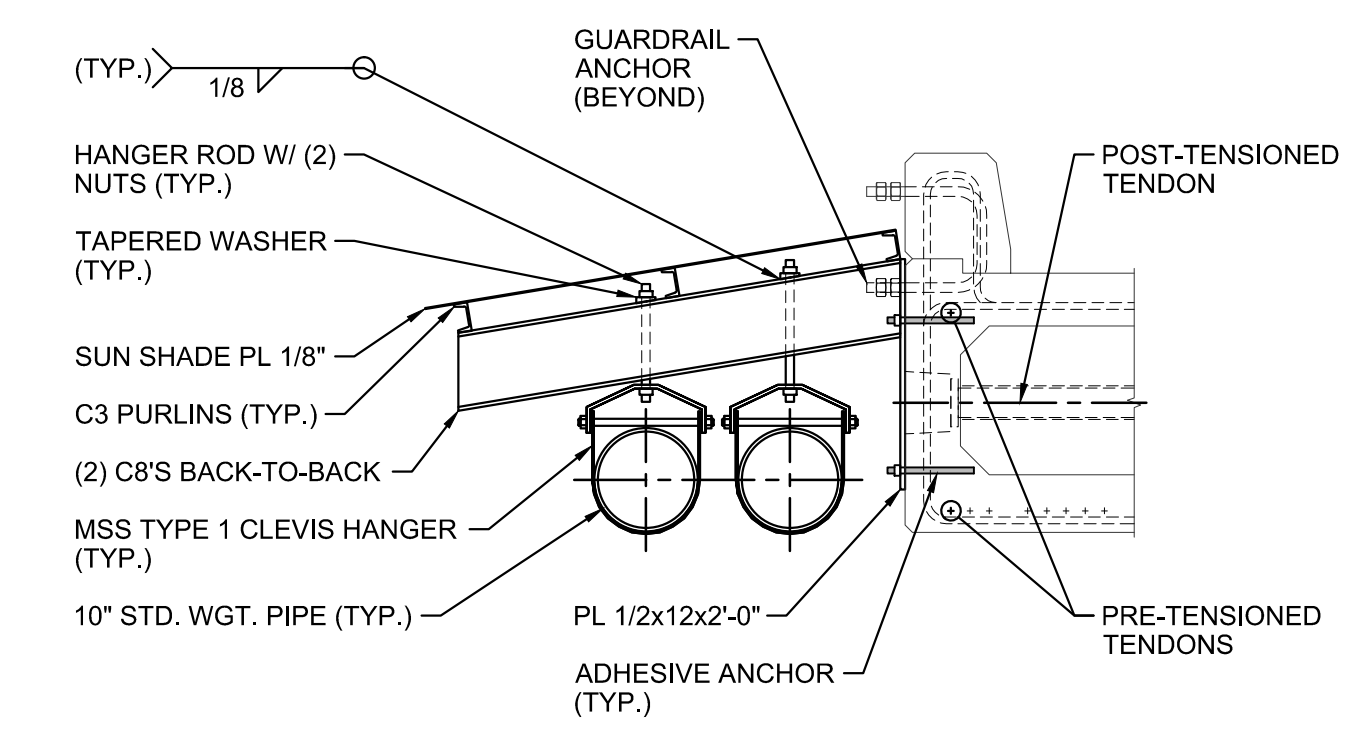
- NOTES**
- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET MUNICIPAL, STATE AND FEDERAL REQUIREMENTS.
 - WHERE HVDC BI-POLE IS INSTALLED IN ROAD SHOULDERS OR OTHERWISE IMPACTS EXISTING PAVED DRIVEWAYS, THE ENTIRE DRIVEWAY APRON SHALL BE REMOVED AND REPLACED.
 - DRIVEWAY MATERIALS SHALL MEET THE CURRENT MUNICIPAL STANDARDS.
 - CULVERTS WITHIN THE EXISTING R.O.W. IMPACTED BY THE CABLE INSTALLATION SHALL BE INSPECTED AND, WHERE APPROPRIATE, REPLACED.

DRIVEWAY APRON PATCH
SCALE: N.T.S.



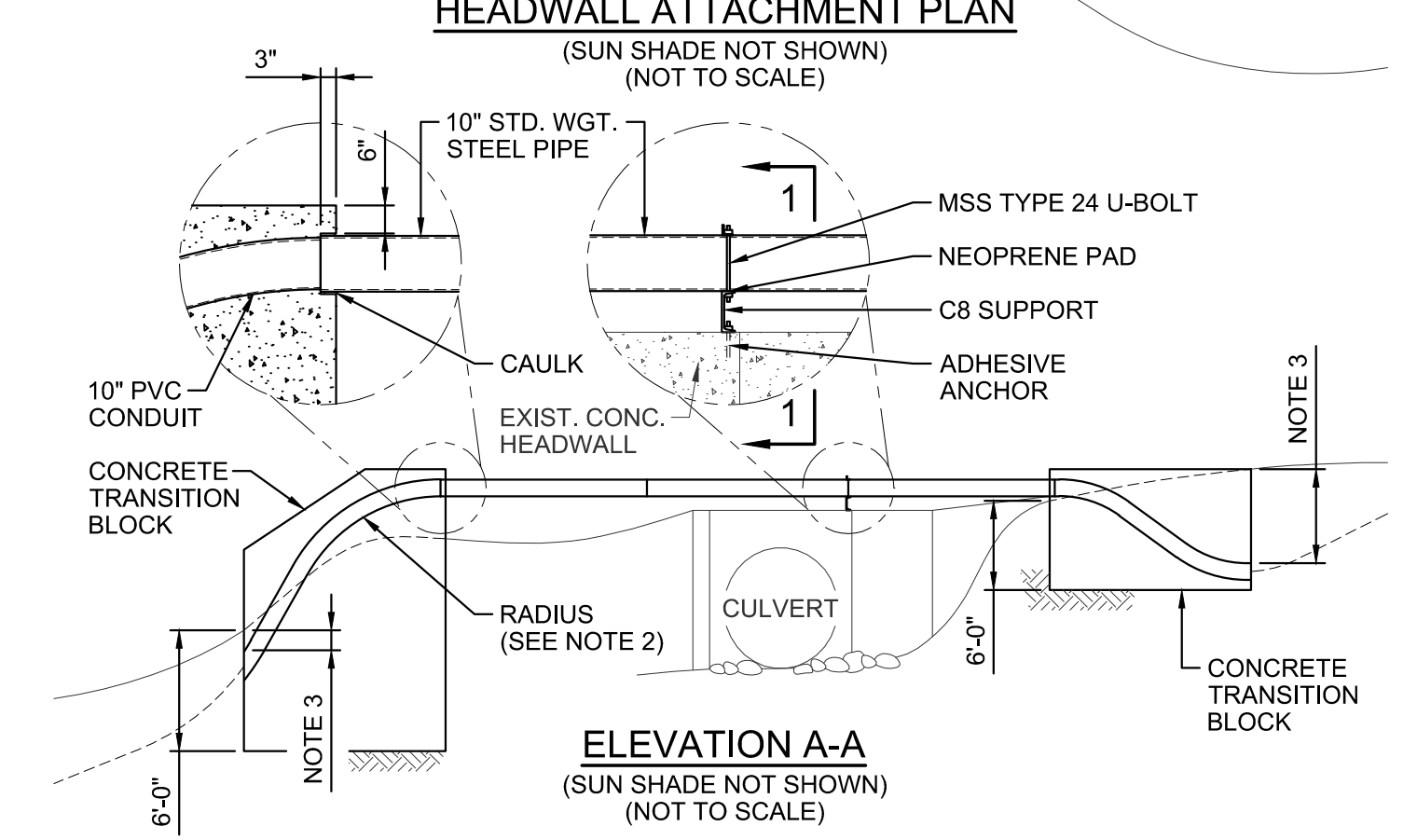
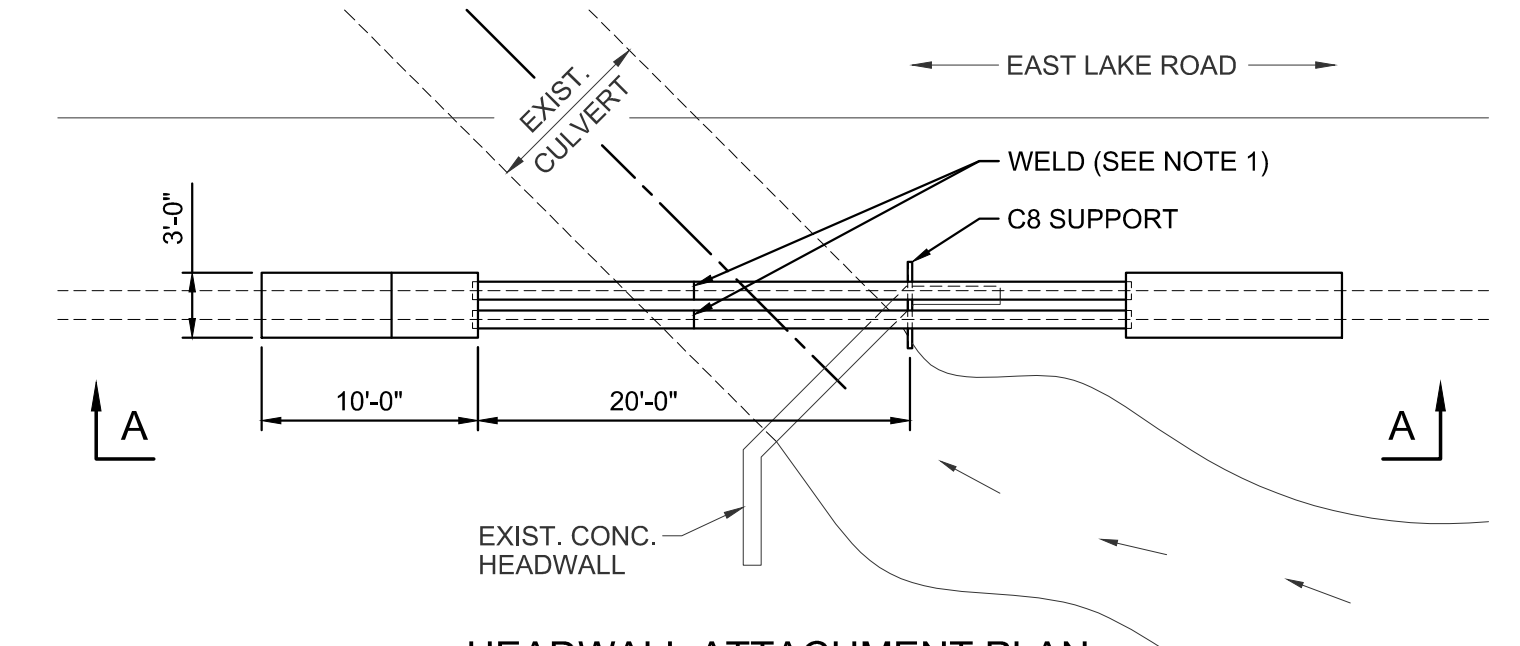
NOTE:
GUARDRAIL AND SUN SHADE NOT SHOWN FOR CLARITY.

EAST LAKE ROAD BRIDGE ATTACHMENT DETAIL 1

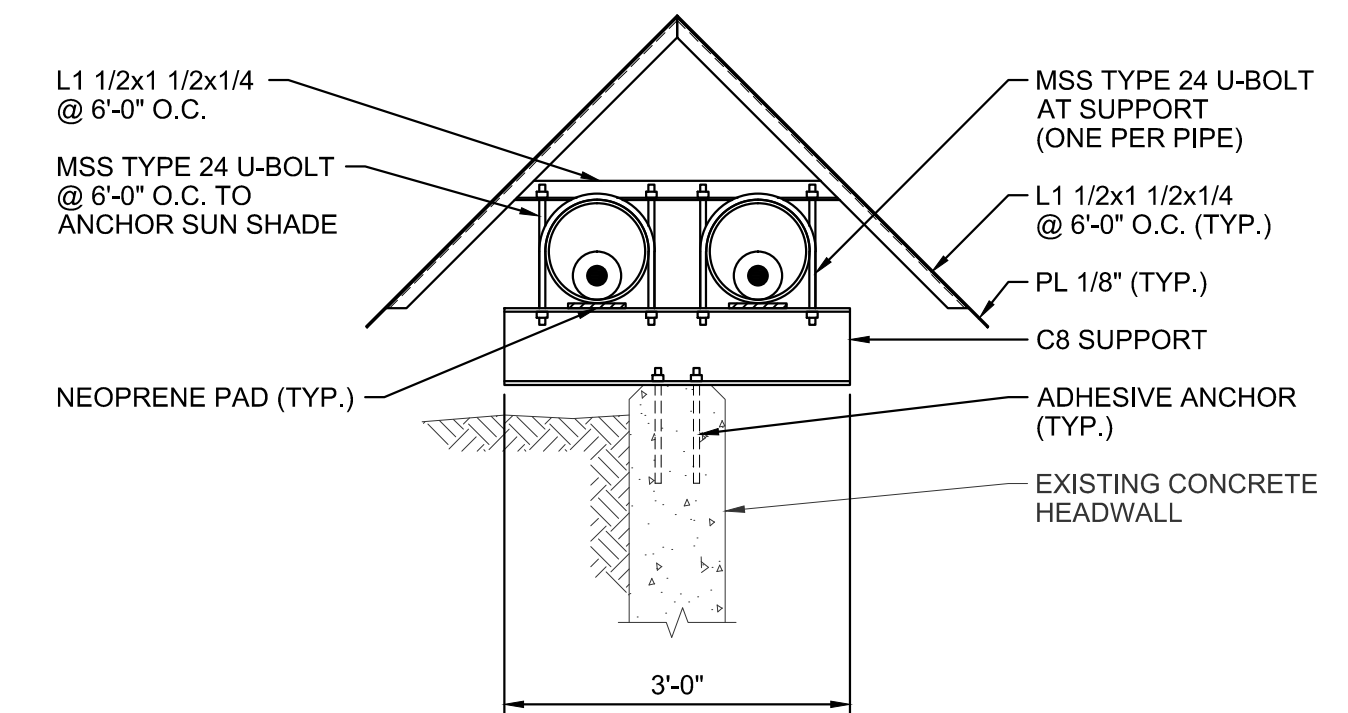


- NOTES**
- REFER TO W.E. DAILEY DESIGN DRAWINGS FOR COORDINATION WITH BRIDGE REINFORCEMENT, PRE- AND POST-TENSIONED TENDONS.
 - CONTRACTOR SHALL AVOID BORING ANCHOR HOLES WITHIN ONE INCH OF PRE- OR POST-TENSIONED TENDONS.
 - CONTRACTOR SHALL AVOID PENETRATING PRE-STRESSED BOX BEAM IN AREAS MARKED "NO DRILL". COORDINATE ANCHOR LOCATIONS WITH REFERENCED W.E. DAILEY DRAWINGS.
 - ALL COMPONENTS OF CONDUIT AND SUPPORTS TO BE GALVANIZED AFTER FABRICATION. FIELD WELD PIPE AFTER FITTING INTO HEADWALL PENETRATIONS. ROOT WELD SHALL BE TIG WELDED WITHOUT BACKER RING. GALVANIZE COMPLETED WELD USING ZINC RICH GALVANIZING PAINT.
 - COORDINATE SUPPORT LOCATION AND SUN SHADE WITH GUARDRAIL ATTACHMENTS. TRIM SUN SHADE PLATE AROUND GUARDRAIL POSTS.
 - CONDUIT APPROACH TO BRIDGE SHALL BE EMBEDDED IN CONCRETE MIN. 6" ALL AROUND CONDUIT.
 - ELEVATION OF BOTTOM OF CONDUIT TO BE AT OR ABOVE THE BRIDGE CORD ELEVATION.

EAST LAKE ROAD BRIDGE ATTACHMENT DETAIL 2

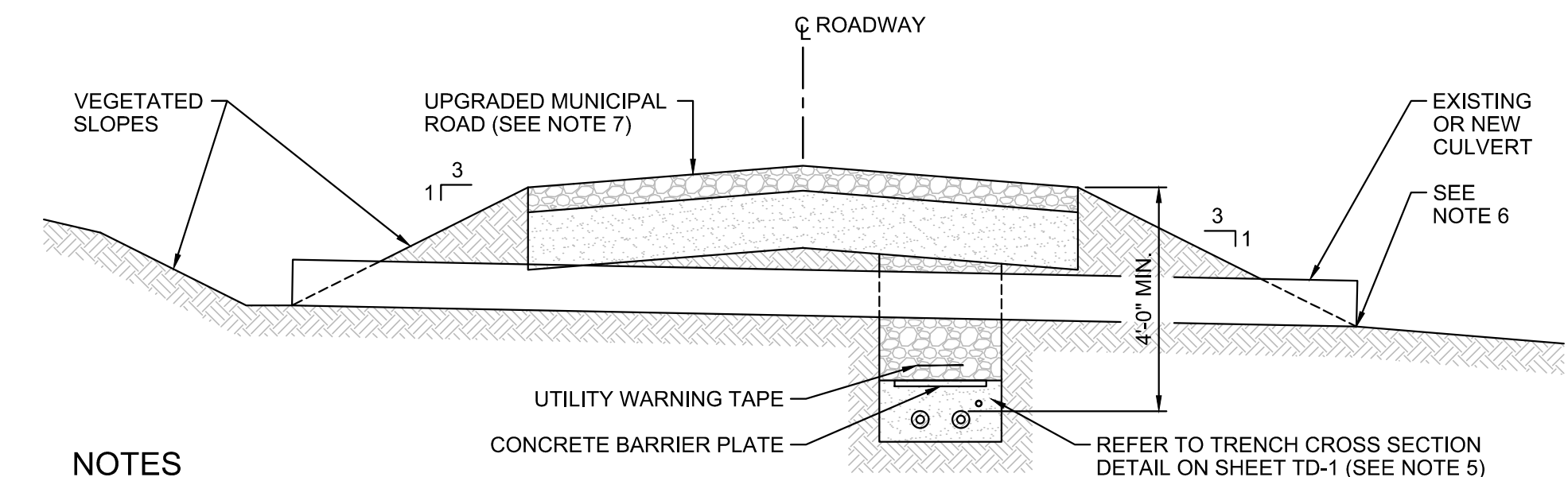


EAST LAKE ROAD HEADWALL ATTACHMENT DETAIL 1
SCALE: N.T.S.



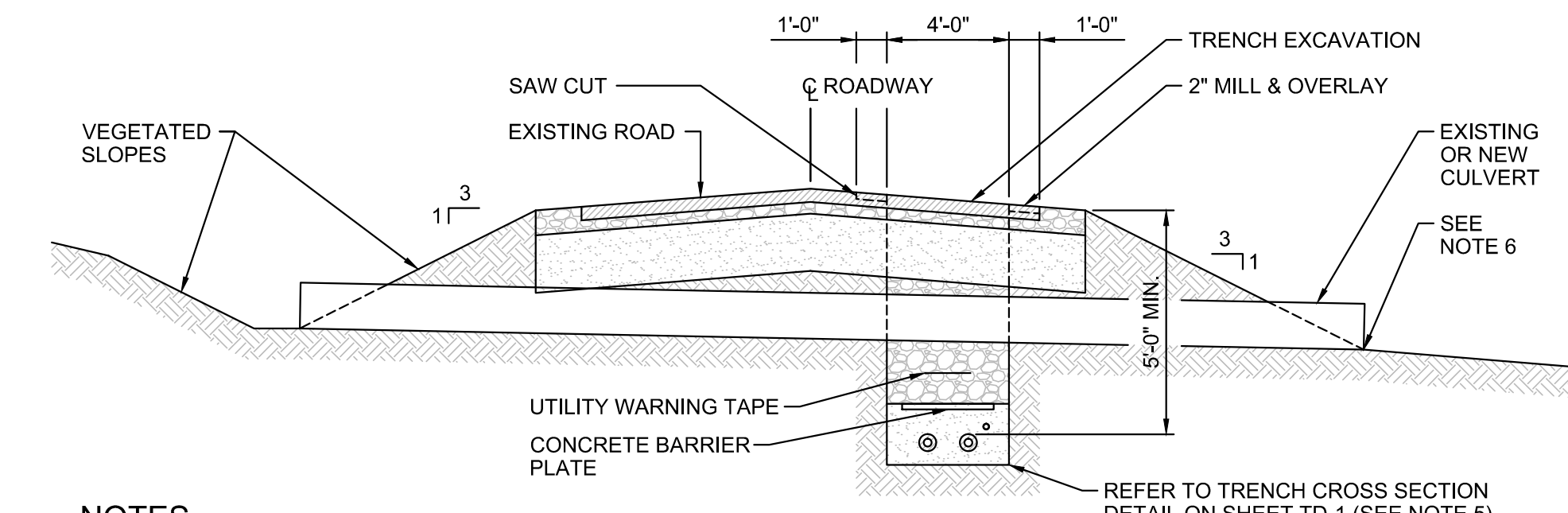
- NOTES**
- ALL COMPONENTS TO BE GALVANIZED AFTER FABRICATION. FIELD WELD PIPE AFTER FITTING INTO TRANSITION BLOCKS. ROOT WELD SHALL BE TIG WELDED WITHOUT BACKER RING. GALVANIZE COMPLETED WELD USING ZINC RICH GALVANIZING PAINT.
 - PVC ENCASED WITHIN CONCRETE SHALL BE BENT AT A 12 FT. RADIUS UNLESS A LARGER RADIUS IS REQUIRED BY THE CABLE MANUFACTURER.
 - CONDUIT BEND RADIUS AND CONCRETE TRANSITION BLOCK LENGTHS SHALL BE COORDINATED TO PROVIDE 4 FEET MINIMUM COVER OVER CABLE AT EXIT FROM BLOCK.

EAST LAKE ROAD HEADWALL ATTACHMENT DETAIL 2



- NOTES**
- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET MUNICIPAL, STATE AND FEDERAL REQUIREMENTS.
 - CULVERTS ALONG THE ROUTE MAY BE DISASSEMBLED OR TEMPORARILY REMOVED TO FACILITATE CABLE INSTALLATION.
 - CULVERTS DETERMINED TO BE UNDERSIZED OR DETERIORATED MAY BE REPLACED.
 - CULVERT BEDDING AND BACKFILL SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE MUNICIPAL ROAD SPECIFICATIONS.
 - CABLE TRENCH DESIGN SHALL BE COORDINATED WITH CULVERT INSTALLATION TO ENSURE NOT LESS THAN 1'-0" OF SEPARATION BETWEEN CULVERT AND HVDC CABLES.
 - UNLESS DETERMINED NECESSARY TO COMPLY WITH THE STREAM ALTERATION PERMIT, CULVERT INVERTS SHALL MATCH EXISTING.
 - EXISTING MUNICIPAL DIRT ROADS SHALL BE UPGRADED TO MEET CURRENT MUNICIPAL ROAD STANDARDS. ROAD WIDENING TO CURRENT MUNICIPAL STANDARDS SHALL BE PROVIDED WHERE PRACTICAL.
 - REFER TO DETAIL PERENNIAL STREAM AT CULVERT CROSSING FOR SEPARATION REQUIREMENTS AT CULVERTS THAT CARRY PERENNIAL STREAMS.

TYPICAL MUNICIPAL CULVERT CROSSING
SCALE: N.T.S.



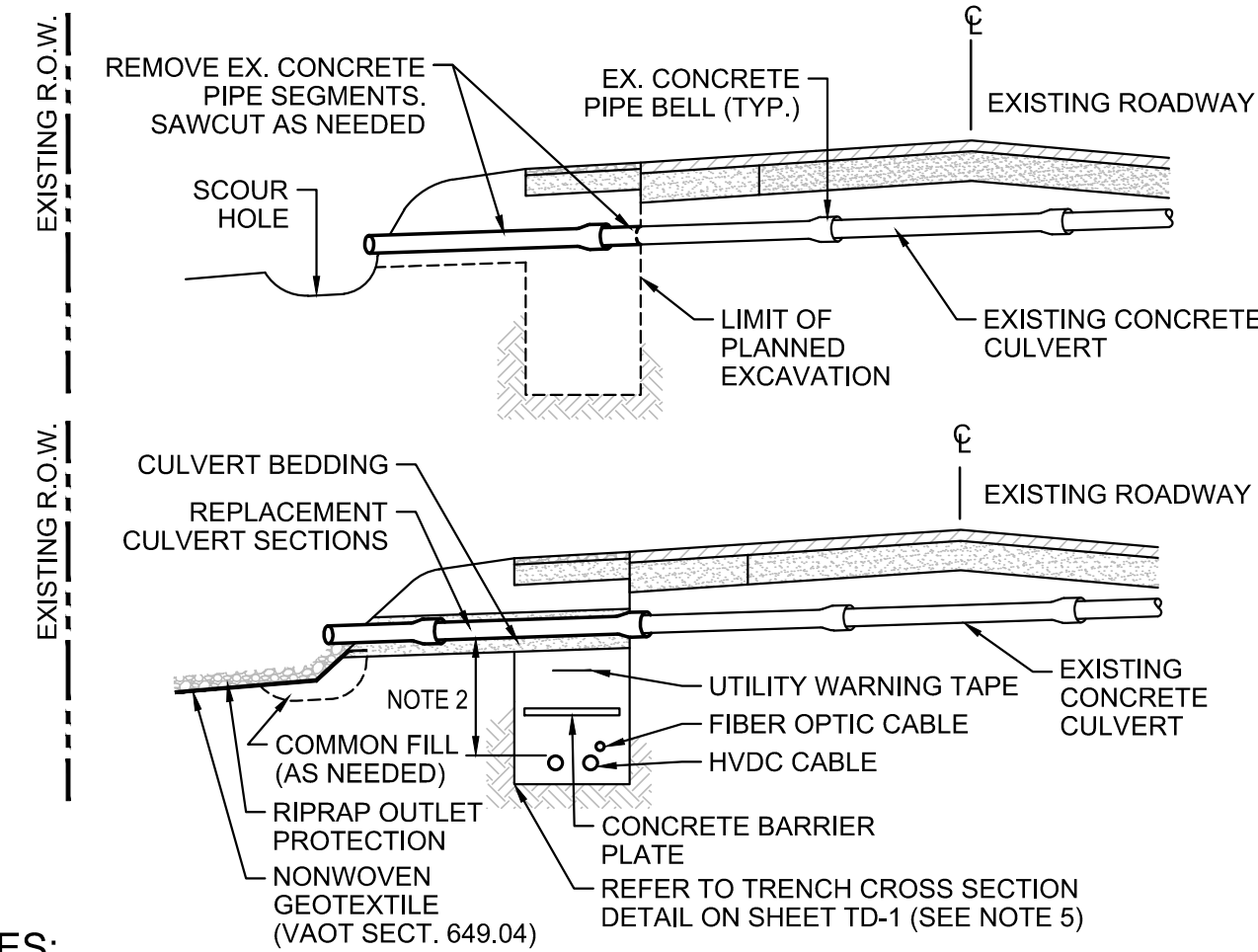
- NOTES**
- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION FOR CONFORMANCE WITH STATE AND FEDERAL REQUIREMENTS.
 - CULVERTS ALONG THE ROUTE MAY BE DISASSEMBLED OR TEMPORARILY REMOVED TO FACILITATE CABLE INSTALLATION.
 - CULVERTS DETERMINED TO BE UNDERSIZED OR DETERIORATED MAY BE REPLACED.
 - CULVERT BEDDING AND BACKFILL SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE STATE ROAD SPECIFICATIONS.
 - CABLE TRENCH DESIGN SHALL BE COORDINATED WITH CULVERT INSTALLATION TO ENSURE NOT LESS THAN 1'-0" OF SEPARATION BETWEEN CULVERT AND HVDC CABLES.
 - UNLESS DETERMINED NECESSARY TO COMPLY WITH THE STREAM ALTERATION PERMIT, CULVERT INVERTS SHALL MATCH EXISTING.
 - CULVERTS CARRYING PERENNIAL STREAMS SHALL COMPLY WITH THE REQUIREMENTS OF THE VT STREAM ALTERATION GENERAL PERMIT.

STATE HIGHWAY CULVERT CROSSING (UNDER)
SCALE: N.T.S.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

No.	Revision	Date	By	Ck	PE	PE #
B	EPSC & PERMITS FOR	3/6/15	TRC	AMW		
C	ISSUED FOR USE	3/27/15	TRC	AMW		
D	STREAM CROSSING DETAILS	4/23/15	TRC	AMW		
E	ISSUED FOR PERMITTING	7/24/15	TRC	AMW		

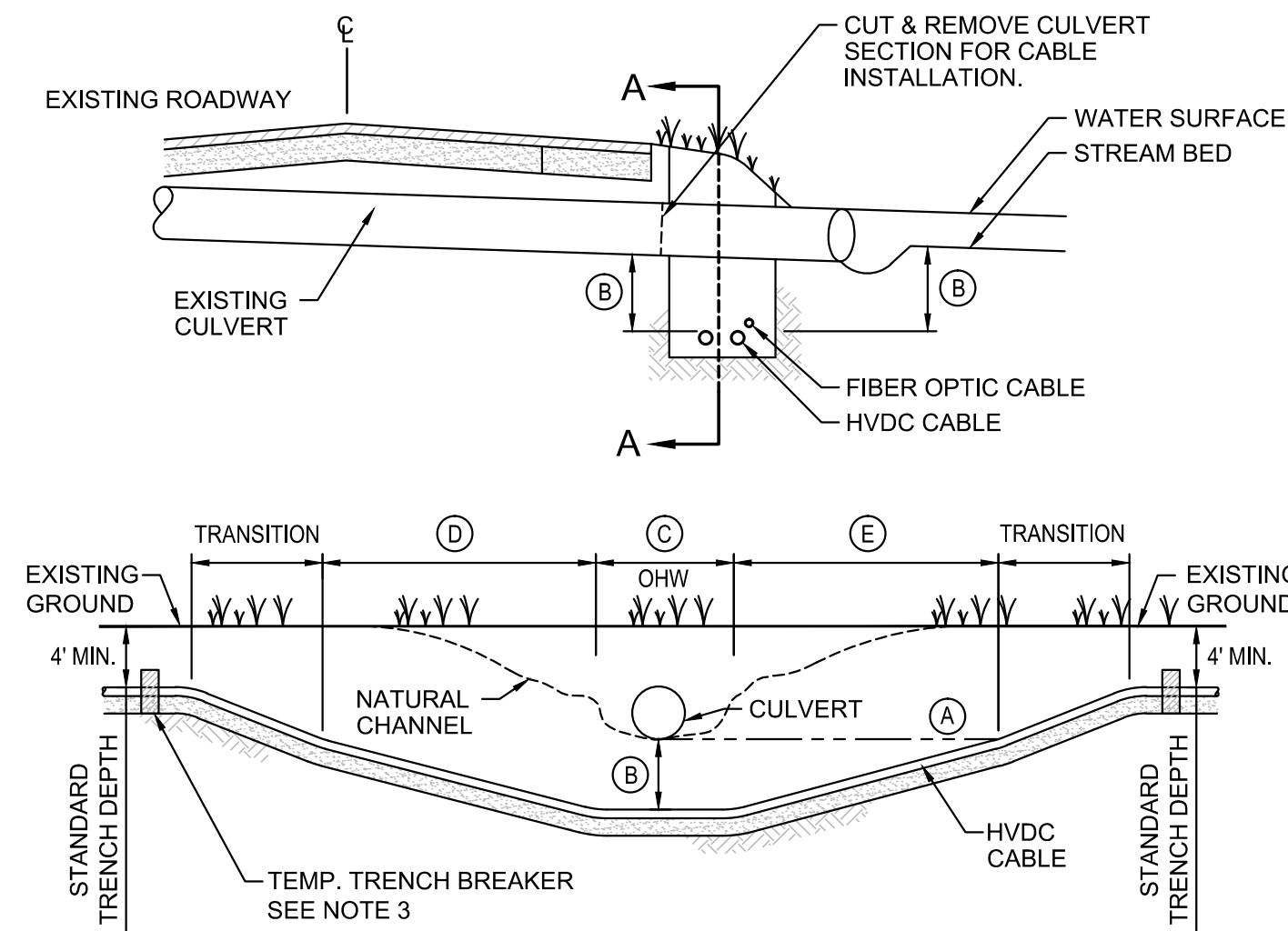
TDI New England
New England Clean Power Link
TDI New England
Typical Details
TD-3
Prepared by: TRC 10/09/14



NOTES:

- CULVERT MODIFICATION TO BE EMPLOYED WHERE EXISTING CULVERT IS OF GOOD SERVICEABLE CONDITION. DEPTH OF CULVERT BURIAL IS 7 FEET OR LESS, AND IT IS DETERMINED FULL REMOVAL IS NOT WARRANTED.
- PROVIDE NEW CULVERT SECTION OR REINSTALL SERVICEABLE SECTIONS. NEW CULVERT SECTIONS SHALL MATCH EXISTING CULVERT DIAMETER AND MATERIAL. PROVIDE NEW CULVERT GASKETS AT EACH BELL.
- CULVERT SECTION LENGTH MAY VARY. CONCEPT DESIGN ASSUMES SECTIONS ARE 10 FEET.
- PROVIDE EROSION REPAIR, RIPRAP, AND GEOTEXTILE AS REQUIRED.
- ALL CULVERT MODIFICATION AND EROSION REPAIR TO BE CONFINED TO ROADWAY R.O.W. UNLESS EASEMENTS OUTSIDE THE R.O.W. HAVE BEEN OBTAINED.
- RIPRAP SHALL BE IN ACCORDANCE WITH VAOT SECT. 613.
- CABLE TRENCH DESIGN SHALL BE COORDINATED WITH CULVERT INSTALLATION TO ENSURE NOT LESS THAN 1'-0" OF SEPARATION BETWEEN CULVERT AND HVDC CABLES.
- REFER TO DETAIL PERENNIAL STREAM AT CULVERT CROSSING FOR SEPARATION REQUIREMENTS AT CULVERTS THAT CARRY PERENNIAL STREAMS.

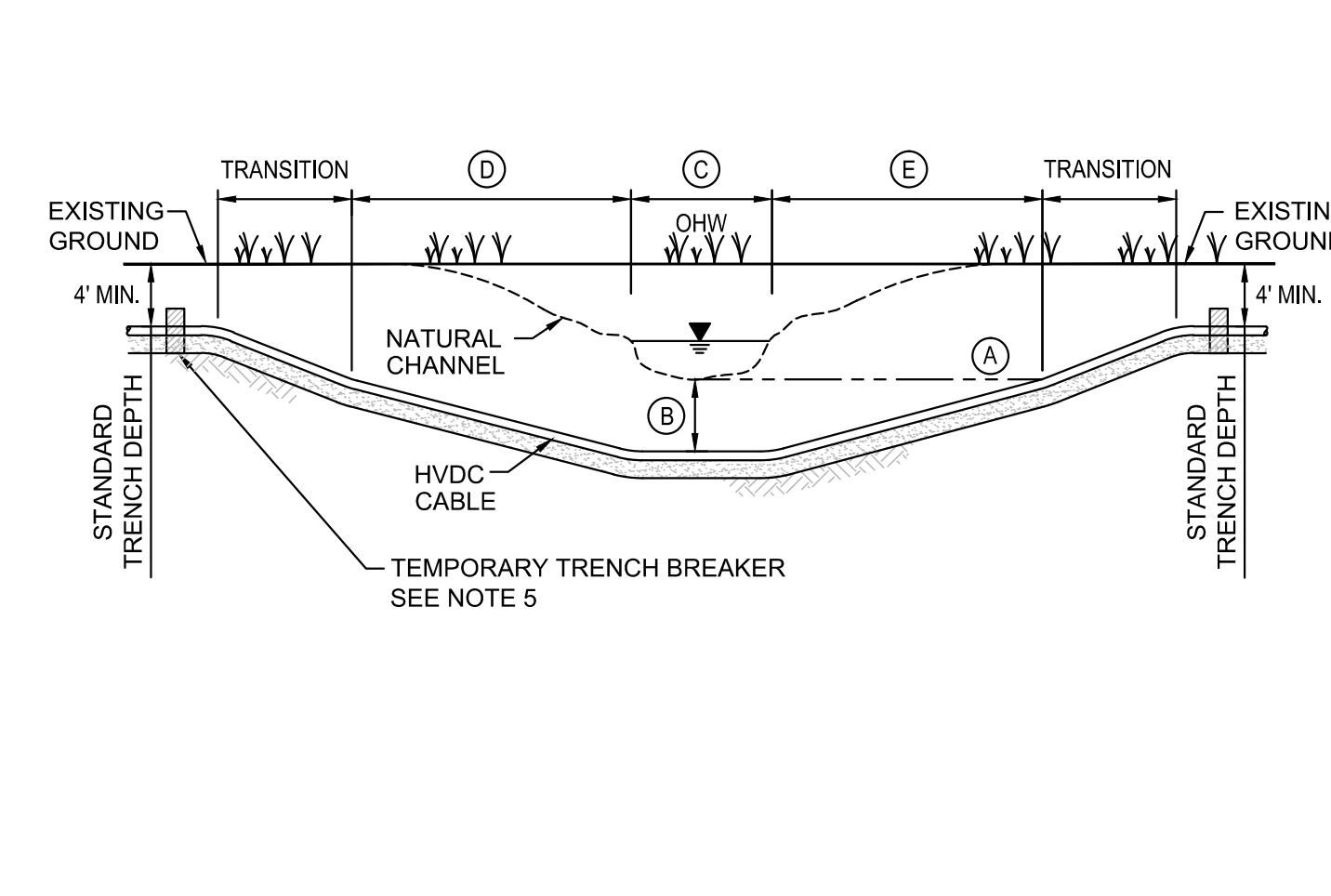
EXISTING CULVERT MODIFICATION - CONCRETE
SCALE: N.T.S.



NOTES:

- REFER TO "PERENNIAL STREAM CROSSING" TABLE FOR STREAM-SPECIFIC DIMENSIONS.
- PROVIDE ENVIRONMENTAL CONTROLS AS SPECIFIED OR DIRECTED PRIOR TO THE START OF "AT CULVERT" CABLE INSTALLATION.
- TEMPORARY TRENCH BREAKER SHALL BE INSTALLED UPGRADIENT FROM THE TRANSITION ZONE ON EACH SIDE OF THE CHANNEL AND REMOVED AS WORK PROGRESSES.
- AT COMPLETION OF CABLE INSTALLATION, UNLESS OTHERWISE INDICATED OR DIRECTED RESTORE CULVERT TO PRE-EXISTING LINES AND GRADES USING NEW OR SUITABLE SALVAGED CULVERT SECTION(S). SECTIONS SHALL BE PERMANENTLY JOINED USING STANDARD CULVERT COUPLINGS OR BELL & SPIGOT GASKET JOINTS AS APPROPRIATE.
- SEE CULVERT MODIFICATION DETAILS.
- RESTORE CULVERT AND EMBANKMENT FOLLOWING CABLE INSTALLATION.

PERENNIAL STREAM AT-CULVERT CROSSING
SCALE: N.T.S.



NOTES:

- REFER TO "PERENNIAL STREAM CROSSING" TABLE FOR STREAM-SPECIFIC DIMENSIONS.
- OPEN TRENCH EXCAVATION OF PERENNIAL STREAMS SHALL BE PERFORMED AFTER ESTABLISHING APPROPRIATE ENVIRONMENTAL CONTROLS AS SPECIFIED AND/OR DIRECTED.
- STREAM BANKS AND BOTTOM SHALL BE RESTORED TO MATCH PRE-CONSTRUCTION CONDITION UNLESS OTHERWISE DIRECTED.
- SEGREGATE AND STOCKPILE STREAM BED AND BANK MATERIALS SEPARATELY FROM SUBSURFACE MATERIAL SOILS. RESTORE SOIL HORIZONS TO THE EXTENT PRACTICABLE WHEN BACKFILLING DISTURBED SECTIONS OF BED AND BANK.
- TEMPORARY TRENCH BREAKER SHALL BE INSTALLED UPGRADIENT FROM THE TRANSITION ZONE ON EACH SIDE OF THE CHANNEL AND REMOVED AS WORK PROGRESSES.

PERENNIAL STREAM OPEN TRENCH CROSSING
SCALE: N.T.S.

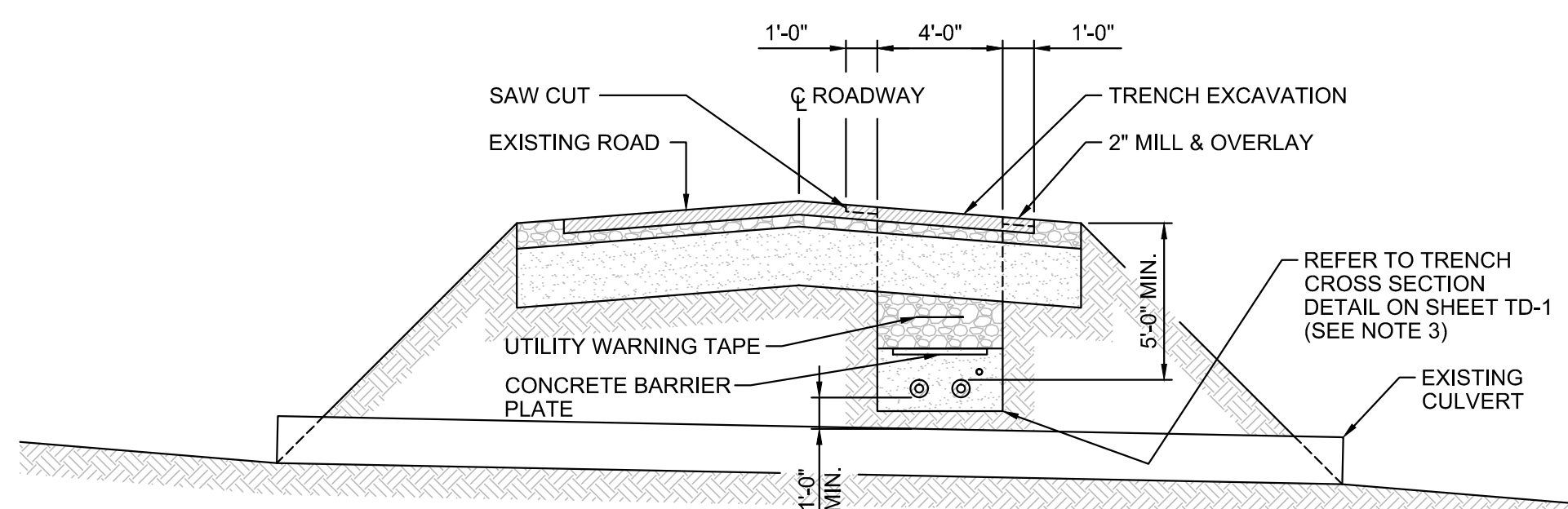
OPEN TRENCH EXCAVATION AND AT - CULVERT CROSSINGS

MILE POST	STREAM ID	CROSSING METHOD	(A)	(B)	(C)	(D)	(E)
99.0	V-BE-AS-3	A/C	318	313	3	20	20
100.7	V-BE-S-8	A/C	472	467	7	10	10
101.2	V-BE-AS-10	A/C	502	497	5	20	20
102.2	V-BE-S-100	OTE	350	345	3	20	20
105.1	V-BE-S-109	OTE	240	235	4	20	20
106.2	V-WH-S-4	OTE	225	220	5	20	20
108.1	V-WH-S-2	A/C	352	347	4.5	20	20
109.6	V-FH-S-17	A/C	335	330	3	20	20
111.0	V-FH-S-5	A/C	379	374	4	20	20
111.8	V-FH-S-10	A/C	400	395	2	20	20
113.2	V-CN-S-101	OTE	420	415	3.5	20	20
117.7	V-CN-S-4	A/C	454	449	5	20	20
119.6	T-IR-S4	A/C	503	498	5	20	20
121.1	T-WR-S34	A/C	524	519	3	20	20
128.7	T-CL-S4	OTE	591	586	15	20	20
132.7	V-SH-S-16	A/C	913	908	3	20	20
135.5	T-SH-S3	OTE	1014	1009	20	50	50
136.1	T-SH-S7	OTE	1064	1059	3	20	20
137.8	V-WA-S-106	OTE	1115	1110	3.5	20	20
137.9	V-WA-S-105	A/C	1129	1124	3	20	20
140.4	T-MH-S28	OTE	1360	1355	25	50	100
141.8	T-MH-AS-23	A/C	1433	1428	4	20	20
142.9	T-MH-AS-20	A/C	1468	1463	4	20	20
143.2	T-MH-AS-45	A/C	1501	1496	5	20	10
144.8	T-MH-S-14	OTE	1391	1386	12	48	48
146.4	T-MH-S1	A/C	1312	1307	7	20	20
150.4	T-LU-S21	A/C	1067	1062	2	5	20
151.5	T-LU-S15	A/C	1404	1399	6	24	24
151.6	T-LU-S12	A/C	1458	1453	4	20	20

NOTES:

- COLUMN HEADINGS IN THE ABOVE TABLE ARE DEFINED AS FOLLOWS (ALL DIMENSIONS IN FEET):
 (A) CULVERT INVERT ELEVATION
 (B) TOP OF CABLE ELEVATION BELOW CULVERT/STREAM INVERT
 (C) INSTALL CABLE 5 FEET (MIN.) BELOW CULVERT/STREAM INVERT FOR FULL STREAM WIDTH AT ORDINARY HIGH WATER (OHW).
 (D) LENGTH OF SUPPLEMENTAL BURIAL DEPTH DOWN-STATION OF STREAM. LENGTH SHALL BE THE GREATER OF 20 FEET OR 4 * OHW UNLESS TRUNCATED BY CONFINING VALLEY WALL.
 (E) LENGTH OF SUPPLEMENTAL BURIAL DEPTH UP-STATION OF STREAM. LENGTH SHALL BE THE GREATER OF 20 FEET OR 4 * OHW UNLESS TRUNCATED BY CONFINING VALLEY WALL.

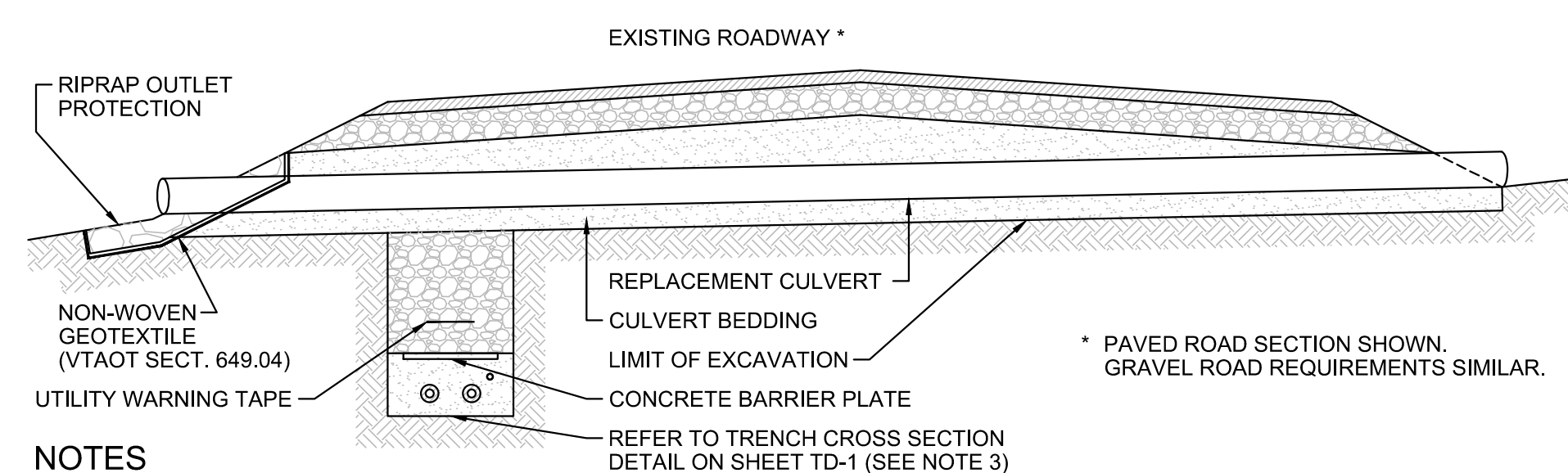
PERENNIAL STREAM CROSSINGS TABLE
SCALE: N.T.S.



NOTES:

- DIMENSIONS AND DETAILS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION FOR CONFORMANCE WITH STATE AND FEDERAL REQUIREMENTS.
- CULVERT BEDDING AND BACKFILL SHALL BE CONSTRUCTED IN ACCORDANCE WITH APPLICABLE MUNICIPAL OR STATE SPECIFICATIONS.
- CABLE TRENCH DESIGN SHALL BE COORDINATED WITH CULVERT INSTALLATION TO ENSURE NOT LESS THAN 1'-0" SEPARATION BETWEEN CULVERT AND BOTTOM OF HVDC CABLE.

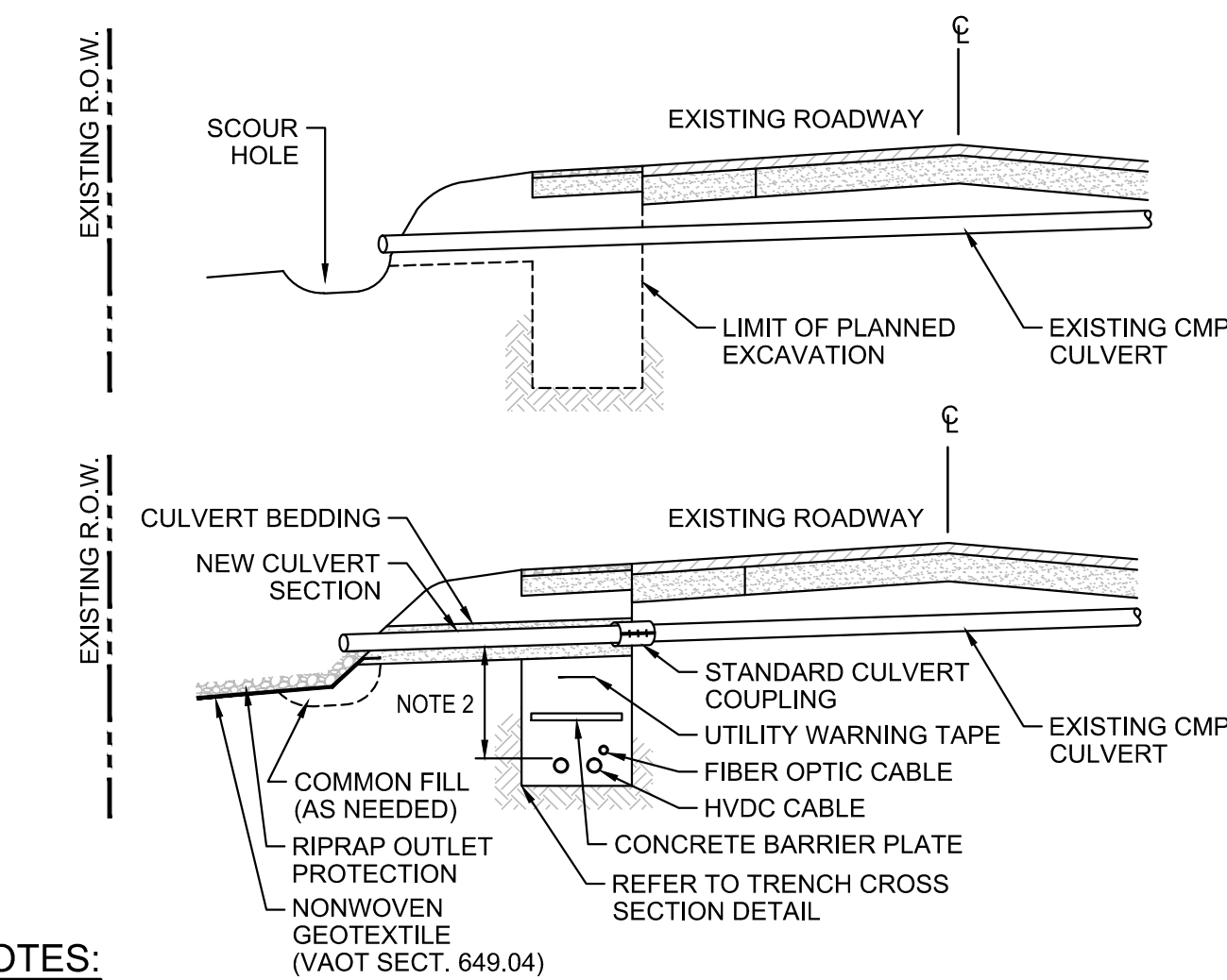
TYPICAL CULVERT CROSSING (OVER)
SCALE: N.T.S.



NOTES:

- CULVERTS MAY BE REPLACED WHEN EXISTING CULVERT IS DETERMINED TO BE UNDERSIZED OR DETERIORATED BEYOND REPAIR.
- NEW CULVERTS SHALL MATCH EXISTING CULVERTS IN DIAMETER UNLESS A LARGER DIAMETER CULVERT IS WARRANTED.
- UNLESS DETERMINED NECESSARY TO COMPLY WITH THE STREAM ALTERATION PERMIT, NEW CULVERT INVERTS SHALL MATCH EXISTING.
- CULVERTS CARRYING PERENNIAL STREAMS SHALL COMPLY WITH THE REQUIREMENTS OF THE VT STREAM ALTERATION GENERAL PERMIT.
- PROVIDE EROSION REPAIR, RIPRAP, AND GEOTEXTILE AS REQUIRED TO MATCH EXISTING INSTALLATION.
- ALL CULVERT INSTALLATIONS AND EROSION REPAIR SHALL BE CONFINED TO ROADWAY R.O.W. UNLESS EASEMENTS OUTSIDE THE R.O.W. HAVE BEEN OBTAINED.
- RIPRAP SHALL BE IN ACCORDANCE WITH VTAOT SECT. 613.
- REFER TO DETAIL TYPICAL ROADWAY DETAILS FOR CABLE TRENCH PROPERTIES AND DIMENSIONS ASSOCIATED WITH CABLE INSTALLATION UNDER PAVEMENT AND SHOULDER OF STATE HIGHWAYS.
- CABLE TRENCH DESIGN SHALL BE COORDINATED WITH CULVERT INSTALLATION TO ENSURE NOT LESS THAN 1'-0" OF SEPARATION BETWEEN CULVERT AND HVDC CABLES.
- REFER TO DETAIL PERENNIAL STREAM AT CULVERT FOR SEPARATION REQUIREMENTS AT CULVERTS THAT CARRY PERENNIAL STREAMS.

TYPICAL CULVERT REPLACEMENT
SCALE: N.T.S.



NOTES:

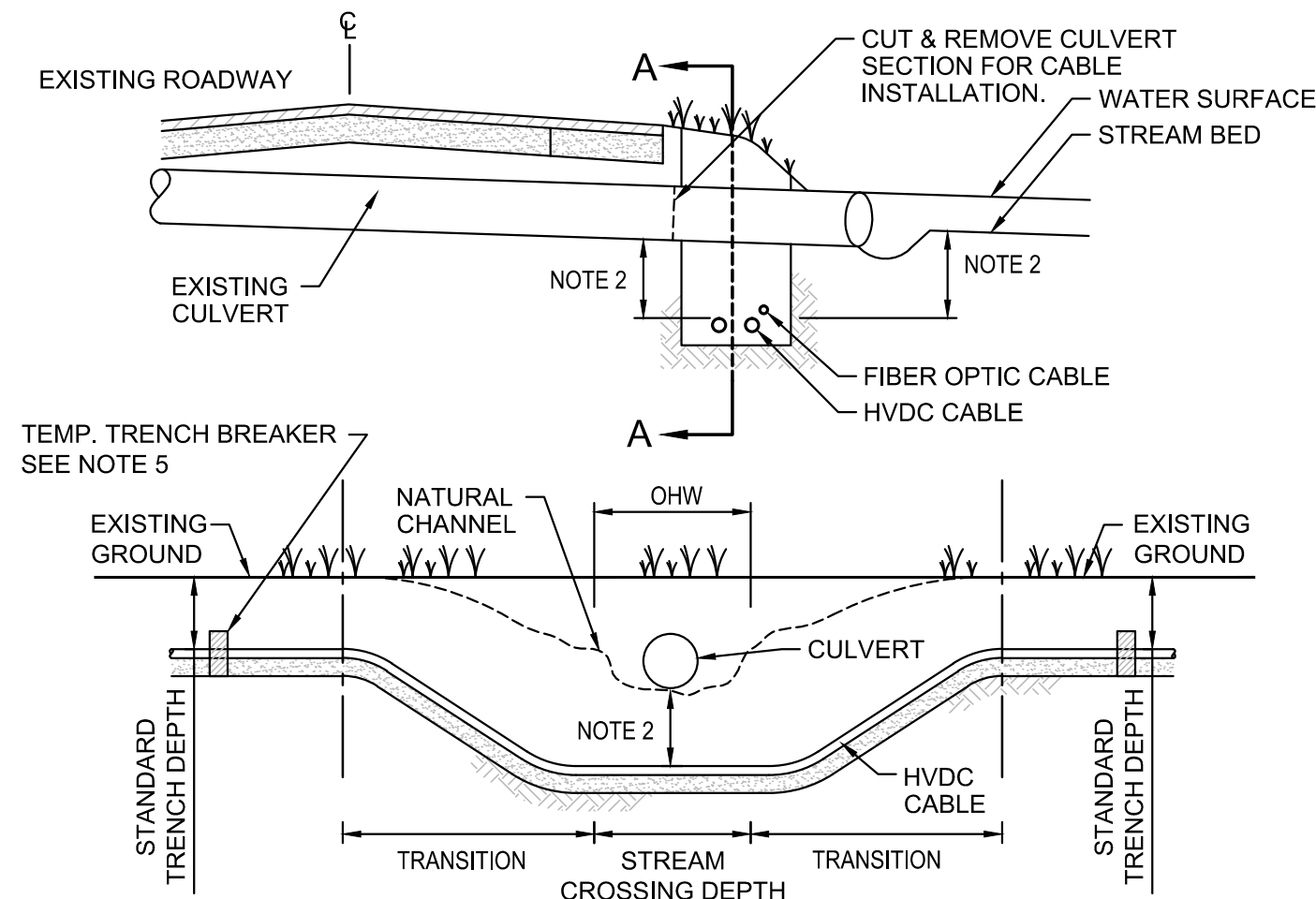
- CULVERT MODIFICATION TO BE EMPLOYED WHERE EXISTING CULVERT IS OF GOOD SERVICEABLE CONDITION. DEPTH OF CULVERT BURIAL IS 7 FEET OR LESS, AND IT IS DETERMINED FULL REMOVAL IS NOT WARRANTED.
- NEW CULVERT SECTION SHALL MATCH EXISTING CULVERT DIAMETER AND MATERIAL. JOIN CULVERT SECTIONS WITH STANDARD GALVANIZED STEEL TWO-PIECE CLAMP TYPE COUPLING.
- PROVIDE EROSION REPAIR, RIPRAP, AND GEOTEXTILE AS REQUIRED.
- ALL CULVERT MODIFICATION AND EROSION REPAIR TO BE CONFINED TO ROADWAY R.O.W. UNLESS EASEMENTS OUTSIDE THE R.O.W. HAVE BEEN OBTAINED.
- RIPRAP SHALL BE IN ACCORDANCE WITH VAOT SECT. 613.
- CABLE TRENCH DESIGN SHALL BE COORDINATED WITH CULVERT INSTALLATION TO ENSURE NOT LESS THAN 1'-0" OF SEPARATION BETWEEN CULVERT AND HVDC CABLES.
- REFER TO DETAIL PERENNIAL STREAM AT CULVERT CROSSING FOR SEPARATION REQUIREMENTS AT CULVERTS THAT CARRY PERENNIAL STREAMS.

EXISTING CULVERT MODIFICATION - CMP
SCALE: N.T.S.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

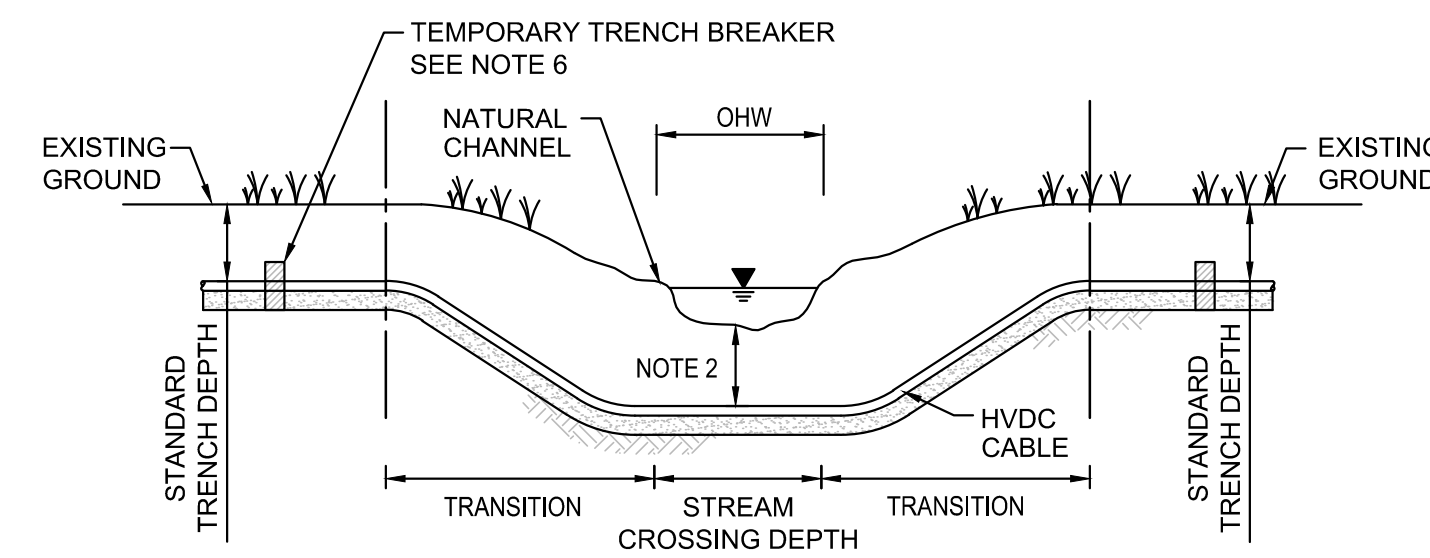
No.	Revision	Date	By	Ck	PE	PE #
B	EPSC & PERMITS IFCR	3/6/15	TRC	AMW		
C	ISSUED FOR USE	3/27/15	TRC	AMW		
D	STREAM CROSSING DETAILS	4/23/15	TRC	AMW		
E	ISSUED FOR PERMITTING	7/24/15	TRC	AMW		

New England Clean Power Link
 TDI New England
 Typical Details
 TD-4
 Prepared by: TRC 10/09/14



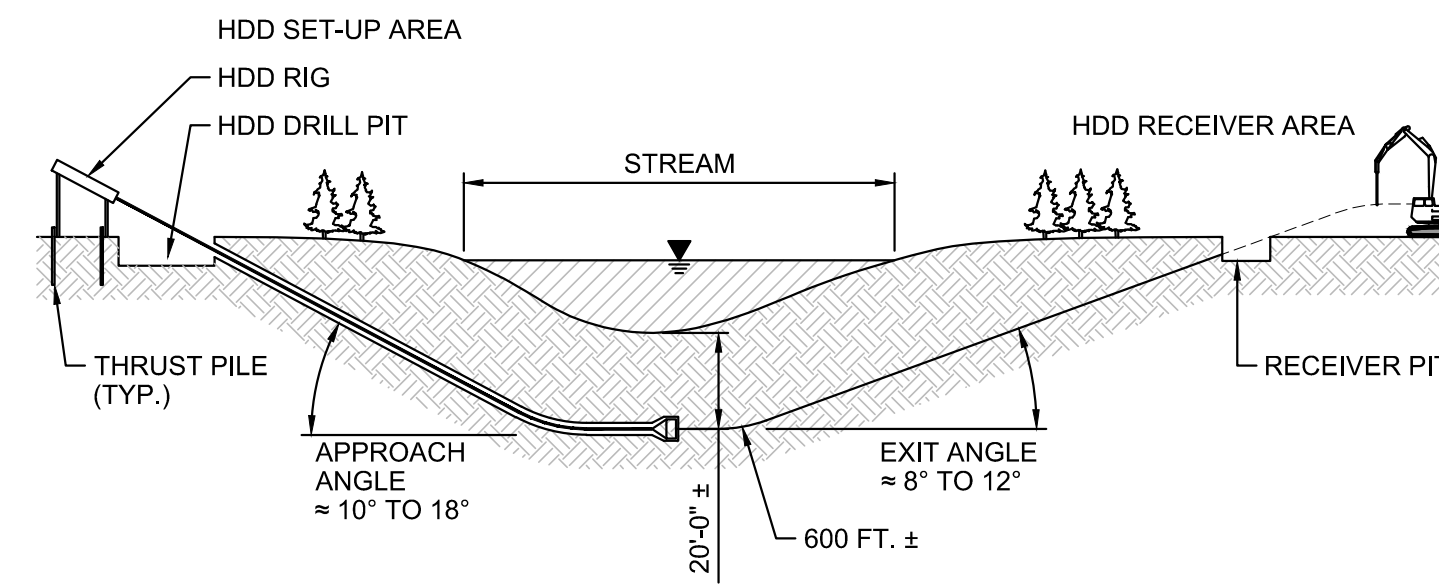
- NOTES:**
1. PROVIDE ENVIRONMENTAL CONTROLS AS SPECIFIED OR DIRECTED PRIOR TO THE START OF "AT CULVERT" CABLE INSTALLATION.
 2. CABLE SHALL BE INSTALLED NOT LESS THAN 5 FEET BELOW EXISTING CULVERT INVERT OR 5 FEET BELOW THE NATURAL STREAM BOTTOM, WHICHEVER IS GREATER.
 3. CABLE INSTALLATION DEPTH SHALL EXTEND NOT LESS THAN THE FULL STREAM BANK WIDTH AT ORDINARY HIGH WATER (OHW).
 4. TRANSITION LENGTH SHALL BE 15 FEET MIN. USING CABLE BEND RADII OF NOT LESS THAN 10 FEET.
 5. TEMPORARY TRENCH BREAKER SHALL BE USED WHEN WORKING IN WET CONDITIONS. TRENCH BREAKER SHALL BE INSTALLED UPGRADIENT FROM THE TRANSITION ZONE ON EACH SIDE OF THE CHANNEL AND REMOVED AS WORK PROGRESSES.
 6. AT COMPLETION OF CABLE INSTALLATION, RESTORE CULVERT TO PRE-EXISTING LINES AND GRADES USING NEW OR SUITABLE SALVAGED CULVERT SECTION(S). SECTIONS SHALL BE PERMANENTLY JOINED USING STANDARD CULVERT COUPLINGS OR BELL & SPIGOT GASKET JOINTS AS APPROPRIATE.
 7. SEE CULVERT MODIFICATION DETAILS.
 8. RESTORE CULVERT AND EMBANKMENT FOLLOWING CABLE INSTALLATION.

INTERMITTENT & EPHEMERAL STREAM AT- CULVERT CROSSING
SCALE: N.T.S.



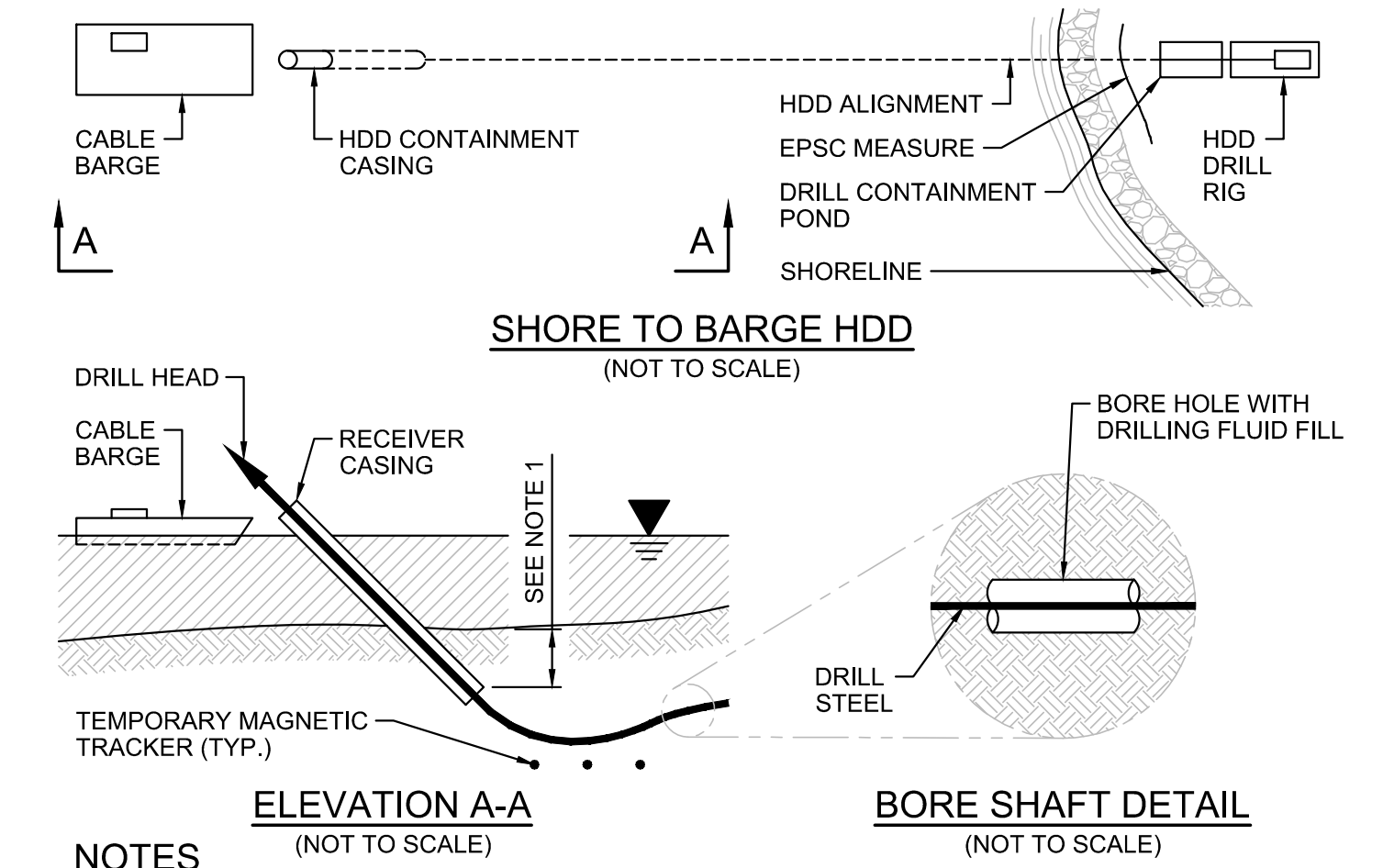
- NOTES:**
1. OPEN TRENCH EXCAVATION OF INTERMITTENT AND EPHEMERAL STREAMS SHALL BE PERFORMED AFTER ESTABLISHING APPROPRIATE ENVIRONMENTAL CONTROLS AS SPECIFIED AND/OR DIRECTED.
 2. AT INTERMITTENT AND EPHEMERAL STREAMS, CABLE SHALL BE INSTALLED NOT LESS THAN 5 FEET BELOW THE EXISTING NATURAL STREAM CHANNEL BOTTOM UNLESS OTHERWISE SPECIFIED OR DIRECTED.
 3. AT INTERMITTENT AND EPHEMERAL STREAMS, THE DEPTH OF INSTALLATION SHALL EXTEND NOT LESS THAN THE FULL STREAM BANK WIDTH AT ORDINARY HIGH WATER (OHW).
 4. TRANSITION LENGTH SHALL BE 15 FEET MIN. USING CABLE BEND RADII OF NOT LESS THAN 10 FEET.
 5. STREAM BANKS AND BOTTOM SHALL BE RESTORED TO MATCH PRE-CONSTRUCTION CONDITION UNLESS OTHERWISE DIRECTED.
 6. SEGREGATE AND STOCKPILE STREAM BED AND BANK MATERIALS SEPARATELY FROM SUBSURFACE MATERIAL SOILS. RESTORE SOIL HORIZONS TO THE EXTENT PRACTICABLE WHEN BACKFILLING DISTURBED SECTIONS OF BED AND BANK.
 7. TEMPORARY TRENCH BREAKER SHALL BE USED WHEN WORKING IN WET CONDITIONS. TRENCH BREAKER SHALL BE INSTALLED UPGRADIENT FROM THE TRANSITION ZONE ON EACH SIDE OF THE CHANNEL AND REMOVED AS WORK PROGRESSES.

INTERMITTENT & EPHEMERAL STREAM OPEN TRENCH CROSSING
SCALE: N.T.S.



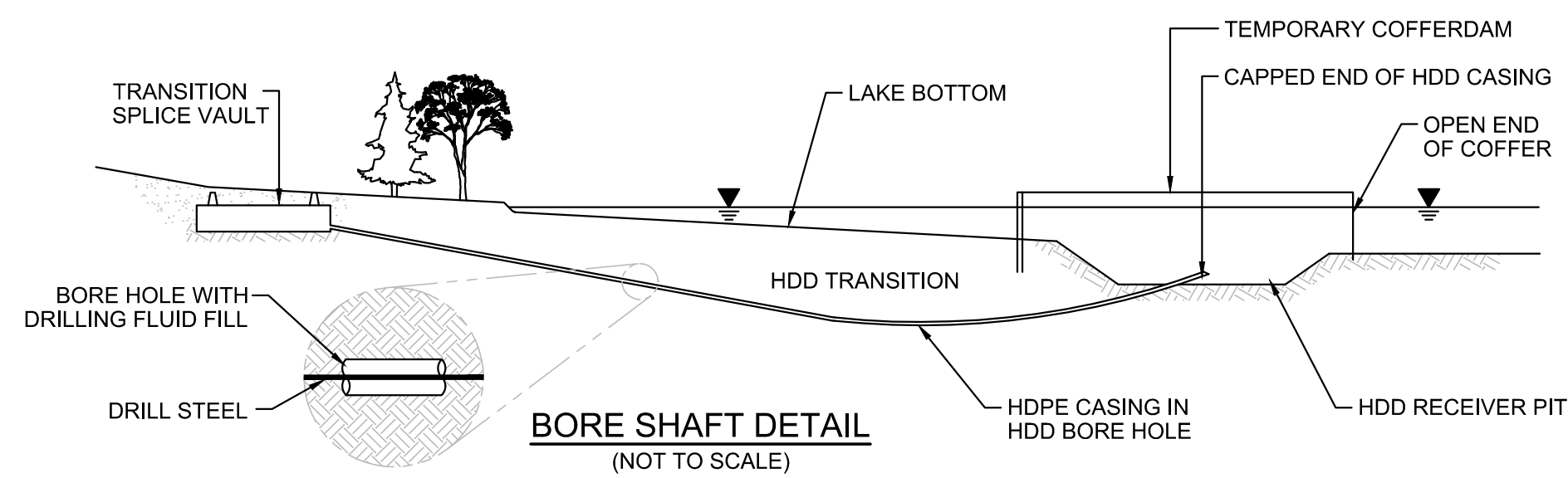
- NOTES:**
1. HDD SET-UP AREA IS APPROXIMATELY 50 FT. x 250 FT. FOR LARGE HDD OPERATIONS. THIS STAGING AREA MAY BE REDUCED FOR SMALLER BORING OPERATIONS OR SOME EQUIPMENT ASSOCIATED WITH LARGE HDD OPERATIONS MAY BE STAGED AT OTHER LOCATIONS.
 2. DRILL PIT MAY BE ELIMINATED IN TOTAL IF ALTERNATE MEANS FOR DRILL MUD CONTAINMENT IS PROVIDED. TYPICAL DRILL PIT FOR LARGE HDD OPERATIONS IS 6 FT. DEEP x 8 FT. x 20 FT.
 3. HDD SHALL PASS NOT LESS THAN 20 FT. UNDER STREAMS NOR LESS THAN 15 FT. BELOW ROADWAYS AND OTHER GROUND SURFACES.
 4. RECEIVER PIT MAY BE ELIMINATED IF ALTERNATE DRILL MUD CONTROL METHOD IS PROVIDED. RECEIVER PIT IS TYPICALLY 5 FT. DEEP x 10 FT. x 10 FT. FOR LARGE DRILL OPERATIONS.
 5. FOR CASING AND CABLE PULL-BACK, CASING MAY BE SUSPENDED ABOVE R.O.W. TO FACILITATE INSTALLATION.
 6. TWO BORE HOLES PER CROSSING ARE REQUIRED. FOR PLANNING PURPOSES, BORE HOLE SPACING SHALL BE 15-25 FEET. LESSER SPACING MAY BE USED IN CERTAIN SOIL CONDITIONS AND/OR BORE OPERATIONS.
 7. REFER TO HORIZONTAL DIRECTIONAL DRILLING INADVERTENT RETURN CONTINGENCY PLAN.

TYPICAL HDD STREAM CROSSING
SCALE: N.T.S.



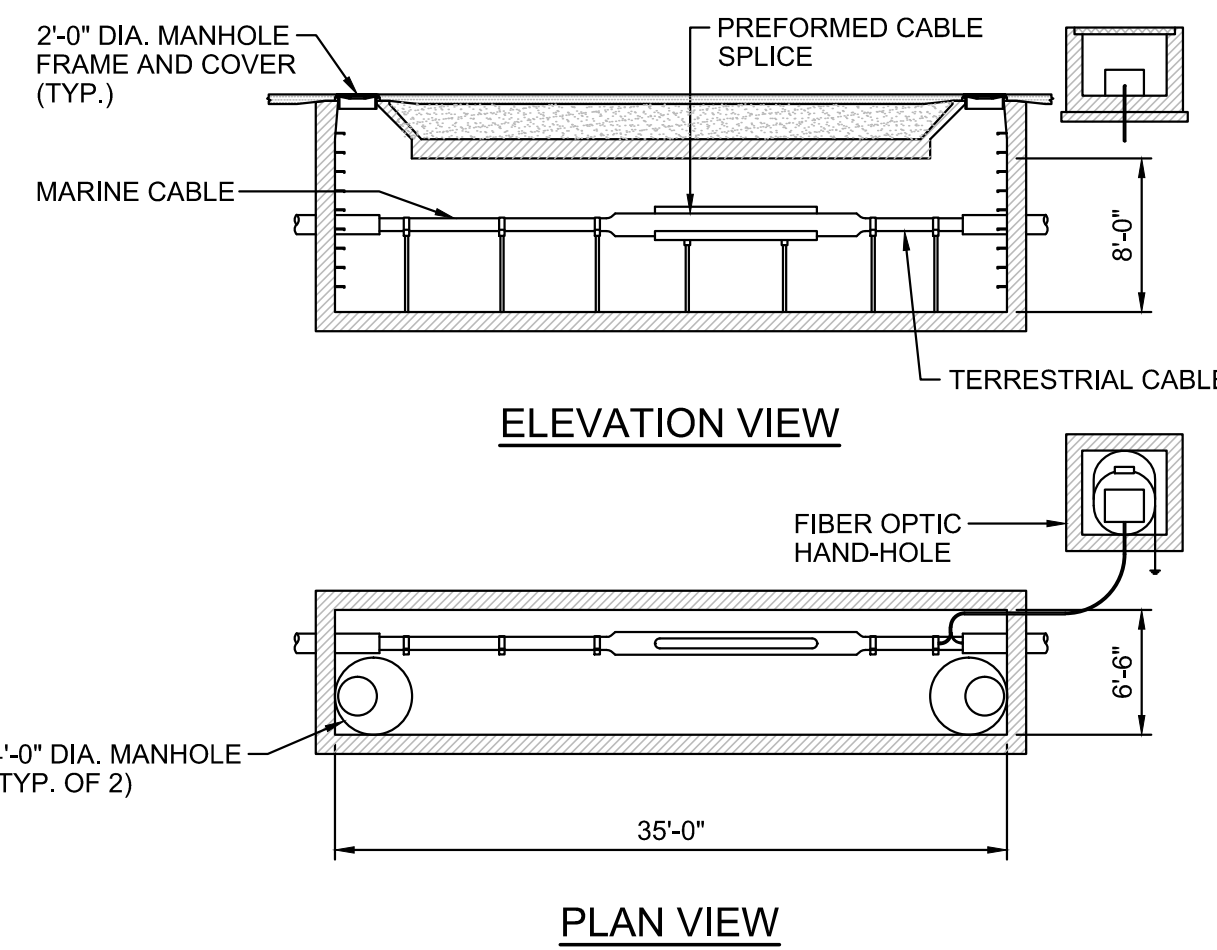
- NOTES:**
1. RECEIVER CASING SHALL BE DRIVEN INTO THE LAKE BOTTOM AT SUFFICIENT DEPTH TO ENSURE ADEQUATE EARTH COVER TO CONTAIN DRILL FLUID.
 2. RECEIVER CASING SHALL BE 48 INCH OR LARGER STEEL PIPE DRIVEN INTO THE LAKE BOTTOM AND USED TO CONTAIN DRILL CUTTINGS AND DRILLING FLUID AT BREAK-OUT.
 3. SUITABLE MAGNETIC TRACKING DEVICES OR SIMILAR SHALL BE USED TO GUIDE THE DRILL LEAD INTO THE RECEIVER CASING.
 4. HDD RECEIVER CASING WILL EXTEND ABOVE THE WATERLINE. EXPOSED STRUCTURE WILL BE MARKED BY BUOYS AND OTHER NAVIGATION AIDS. A NOTICE TO MARINERS WILL BE ISSUED WHEN APPROPRIATE.
 5. RECEIVER CASING AND TRACKING DEVICES SHALL BE REMOVED AT THE COMPLETION OF THE HDD OPERATION.
 6. CABLE BARGE WILL BE USED FOR HDD TOOL INSTALLATION/REMOVAL, CASING PULL-IN, AND CABLE PULLING.
 7. DRILLING FLUID IS TYPICALLY BENTONITE DRILLING MUD. WATER MAY BE USED UNDER SOME CIRCUMSTANCES, INCLUDING THE LAST 10 FEET OF BORING PRIOR TO BREAKING THROUGH THE LAKE BOTTOM.
 8. COFFER DAM MAY BE USED IN LIEU OF RECEIVER CASING SHOULD BOTTOM CONDITIONS OR OTHER FACTORS NOT BE CONDUCIVE TO RECEIVER INSTALLATION OR USE. REFER TO COFFERDAM DETAIL.

HDD RECEIVER CASING
SCALE: N.T.S.



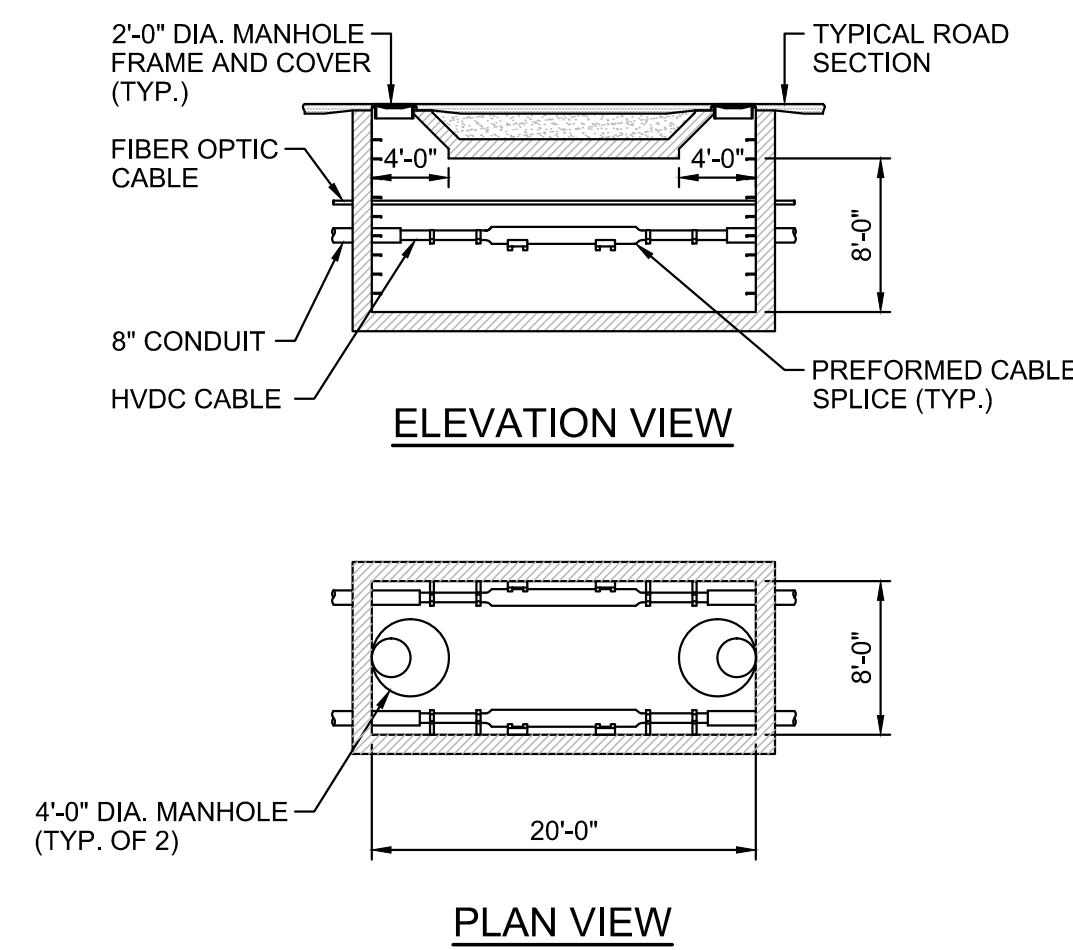
- NOTES:**
1. COFFERDAM TO BE UTILIZED WHERE NECESSARY TO STABILIZE BOTTOM SEDIMENT AT HDD TERMINUS.
 2. PILES SHALL BE REMOVED AT COMPLETION OF CABLE INSTALLATION IN COORDINATION WITH BMP REQUIREMENTS.
 3. COFFERDAM WILL EXTEND ABOVE THE WATERLINE. EXPOSED STRUCTURE WILL BE MARKED BY BUOYS AND OTHER NAVIGATION AIDS. A NOTICE TO MARINERS WILL BE ISSUED WHEN APPROPRIATE.
 4. DRILLING FLUID IS TYPICALLY BENTONITE DRILLING MUD. WATER MAY BE USED UNDER SOME CIRCUMSTANCES, INCLUDING THE LAST 10 FEET OF BORING PRIOR TO BREAKING THROUGH THE LAKE BOTTOM.
 5. IN LIEU OF COFFERDAM INSTALLATION, AN HDD RECEIVER CASING MAY BE USED. REFER TO RECEIVER CASING DETAIL.

HDD COFFERDAM INSTALLATION
SCALE: N.T.S.



- NOTES:**
1. SPLICE VAULTS TO BE CONSTRUCTED IN IMMEDIATE VICINITY OF MARINE CABLE LANDFALL LOCATION. ONE SPLICE VAULT PER BI-POLE CONDUCTOR WILL BE REQUIRED.
 2. ONLY ONE FIBER CABLE SPLICE HAND-HOLE WILL BE REQUIRED.
 3. SPLICE VAULT DESIGN AND DIMENSIONS ARE CONCEPT ONLY. ACTUAL INSTALLED DIMENSIONS AND CONFIGURATION MAY DIFFER.

TYPICAL TRANSITION SPLICE VAULT



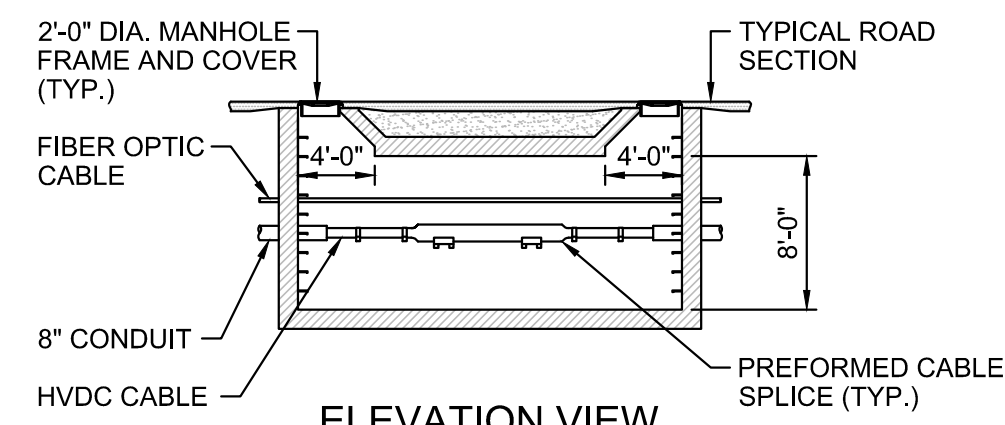
- NOTES:**
1. SPLICE MANHOLE FRAMES AND COVERS SHALL BE RATED FOR H-20 VEHICLE LOADING.
 2. SPLICE MANHOLE DESIGN AND DIMENSIONS ARE CONCEPT ONLY. INSTALLED LOCATION, DIMENSIONS AND DESIGN MAY DIFFER FROM THAT PRESENTED HEREIN.

TYPICAL HVDC SPLICE MANHOLE

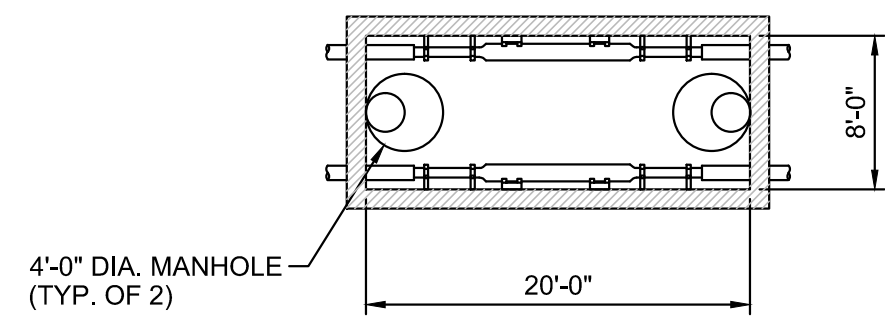
Designed	TRC
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Checked	-
Approved	-
Scale	AS NOTED

No.	Revision	Date	By	Ck	PE	PE #
B	EPSC & PERMITS IFCR	3/6/15	TRC	AMW		
C	ISSUED FOR USE	3/27/15	TRC	AMW		
D	STREAM CROSSING DETAILS	4/23/15	TRC	AMW		
E	ISSUED FOR PERMITTING	7/24/15	TRC	AMW		

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Typical Details



ELEVATION VIEW



PLAN VIEW

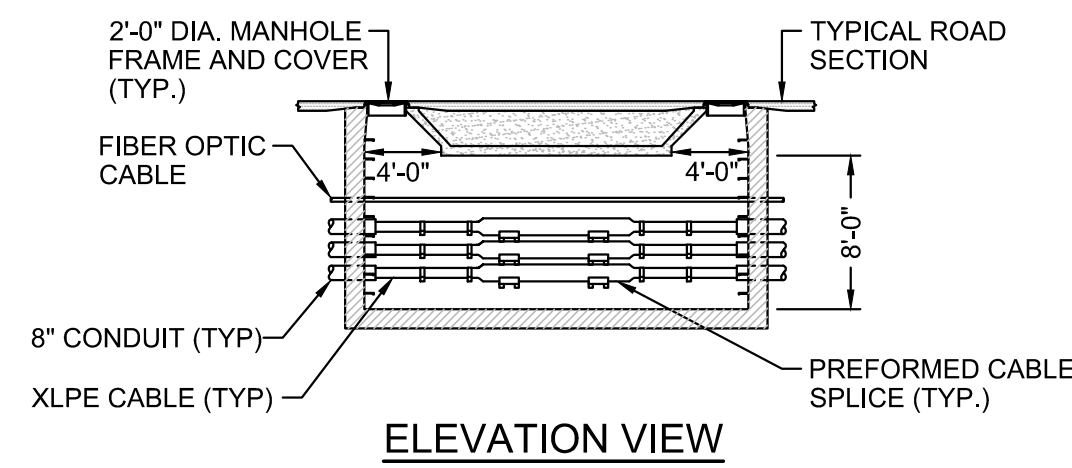
NOTES

1. SPLICE MANHOLE FRAMES AND COVERS SHALL BE RATED FOR H-20 VEHICLE LOADING.
2. SPLICE MANHOLE DESIGN AND DIMENSIONS ARE CONCEPT ONLY. INSTALLED LOCATION, DIMENSIONS AND DESIGN MAY DIFFER FROM THAT PRESENTED HEREIN.

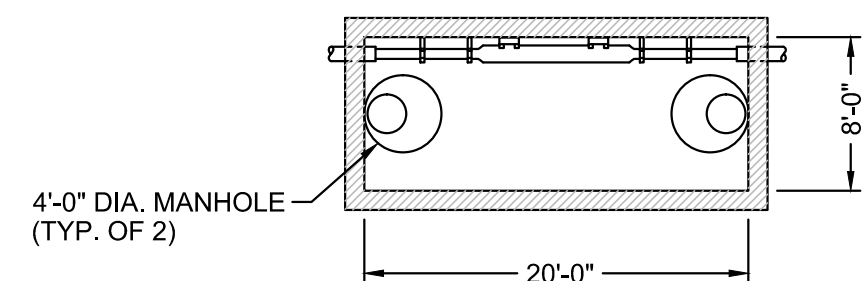
TYPICAL HVDC SPLICE MANHOLE

TYPICAL DAM & PUMP STREAM CROSSING

SCALE: N.T.S.



ELEVATION VIEW



PLAN VIEW

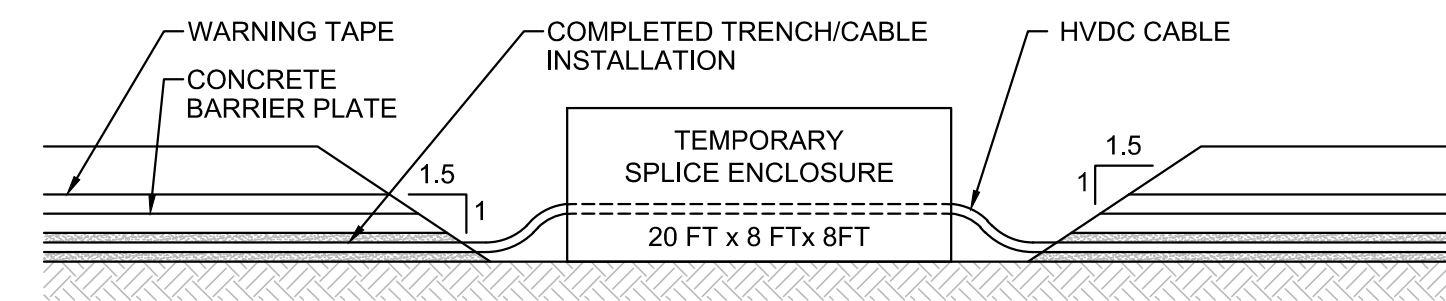
NOTES

1. 345 KV HVAC SPLICE MANHOLE SHALL BE USED FOR THE ROUTE SEGMENT BETWEEN THE NECP CONVERTER STATION AND THE EXISTING COOLIDGE SUBSTATION.
2. ONE OR MORE SPLICE MANHOLES WILL BE INSTALLED WITHIN THE CONVERTER STATION ACCESS ROAD AND NELSON ROAD.
3. SPLICE MANHOLE, FRAMES AND COVERS SHALL BE RATED FOR H-20 VEHICLE LOADING.
4. SPLICE MANHOLE DESIGN AND DIMENSIONS ARE CONCEPT ONLY. INSTALLED LOCATION, DIMENSIONS AND DESIGN MAY DIFFER FROM THAT PRESENTED HEREIN.

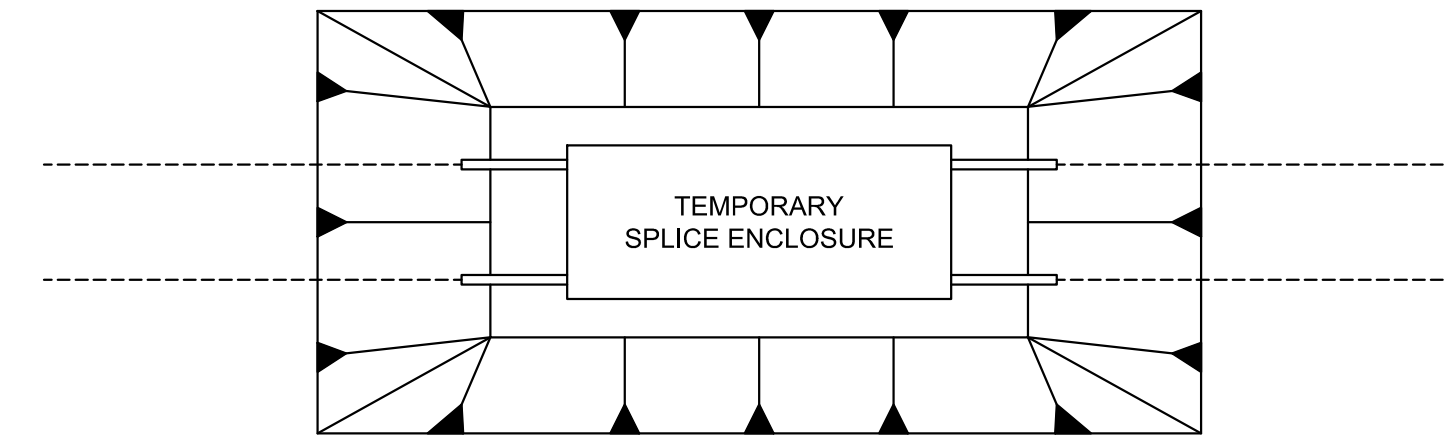
HVAC SPLICE MANHOLE

DEWATERING DETAIL AT CULVERT CROSSING

SCALE: N.T.S.



ELEVATION VIEW
(NOT TO SCALE)



SPLICE ENCLOSURE PLAN VIEW
(NOT TO SCALE)

NOTES

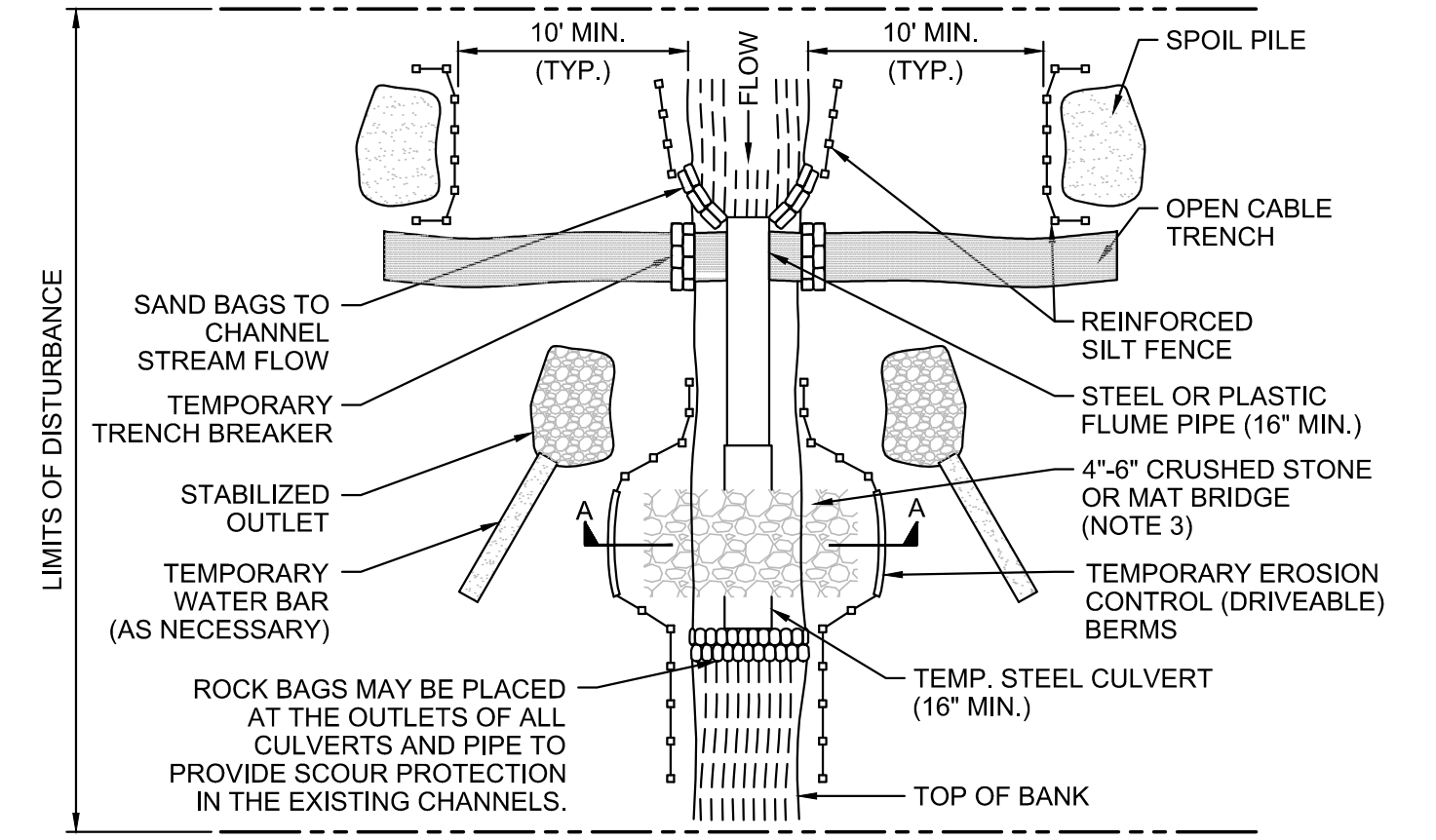
1. HVDC CABLE SHALL BE DIRECT BURIED AFTER SPLICING. WORK SEQUENCE INCLUDES:
 - A. LAY CABLES WITH APPROXIMATELY 20 FEET OF OVERLAP AT PLANNED SPLICE LOCATIONS.
 - B. BACKFILL OR PLATE OVER TRENCH UNTIL SPLICING OPERATION REACHES THIS LOCATION.
 - C. EXPOSE LAPPED CABLES AND EXCAVATE TEMPORARY SPLICE PIT WITH BOTTOM DIMENSION OF APPROXIMATELY 28 FEET X 10 FEET.
 - D. INSTALL TEMPORARY CLIMATE CONTROLLED SPLICE ENCLOSURE WITHIN PIT.
 - E. LIFT CABLES INTO ENCLOSURE USING FLOOR ACCESS HATCH AND PERFORM SPLICES.
 - F. WHEN SPLICE IS COMPLETE RETURN CABLE TO FLOOR OF EXCAVATION AND REMOVE SPLICE ENCLOSURE.
 - G. BACKFILL EXCAVATION, INCLUDING PLACEMENT OF CONCRETE BARRIER AND WARNING TAPE OVER CABLES.
2. SPLICE PIT SHOWN IS FOR CONCEPT ONLY. ACTUAL DIMENSIONS AND DEPTH MAY VARY BASED ON SPECIFIC SPLICE LOCATION AND ENCLOSURE.
3. EXCAVATION DEPICTED USES SIDES SLOPED AT 1 1/2 : 1. IN LIEU OF SLOPED EXCAVATION, SHORING AND OTHER MEANS MAY BE USED TO LIMIT SIZE OF EXCAVATION.

HVDC SPLICE SEQUENCE

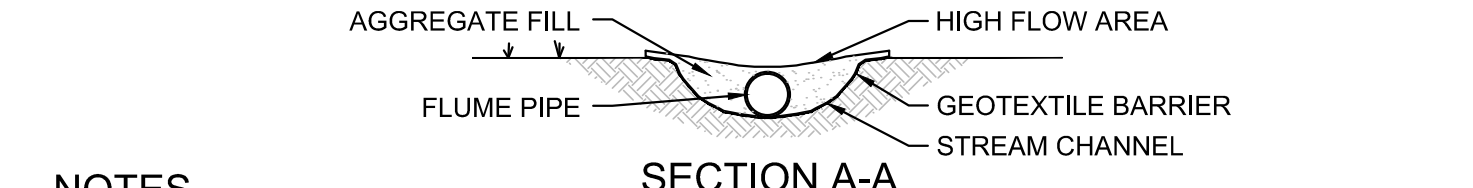
SCALE: N.T.S.

CONSTRUCTION STORAGE AREAS

SCALE: N.T.S.



PLAN VIEW



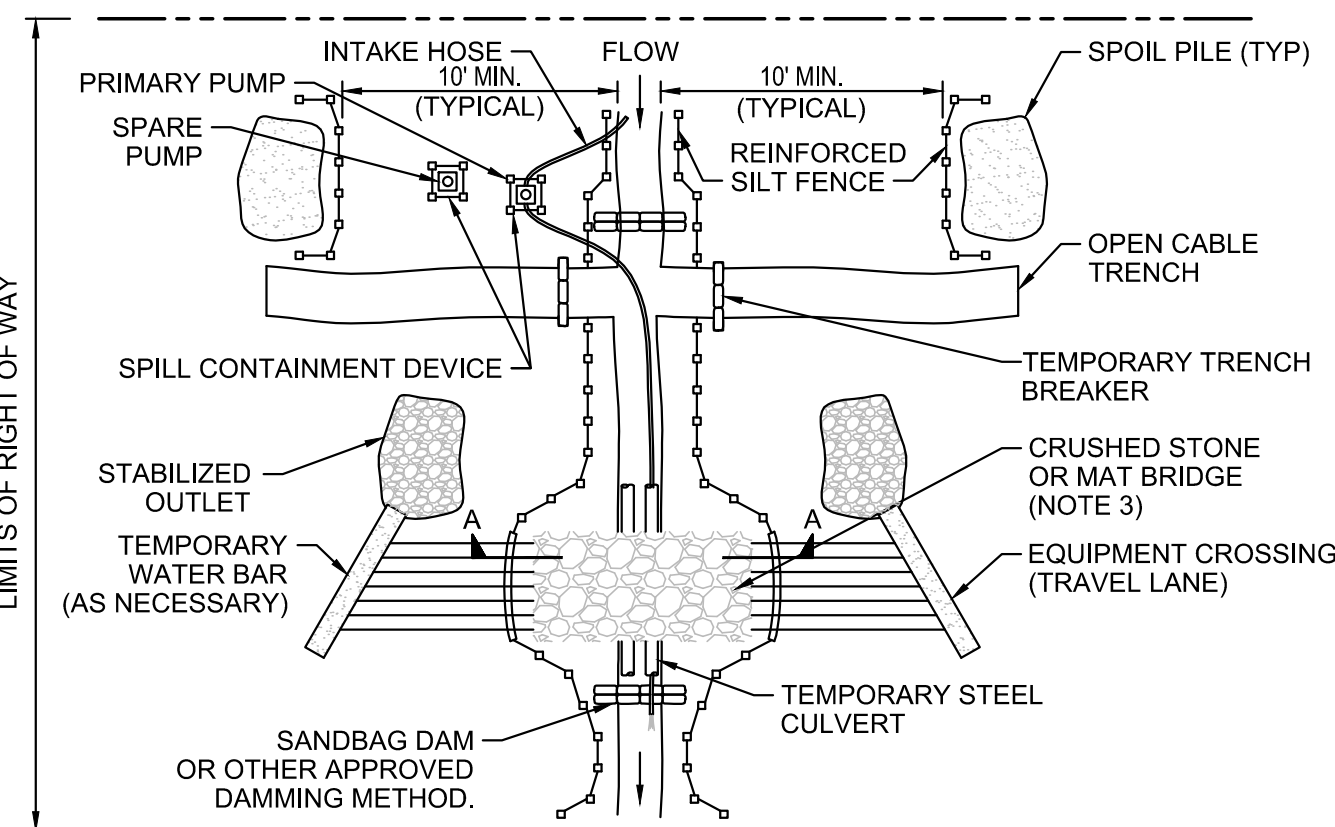
SECTION A-A

NOTES

1. DIMENSIONS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET MUNICIPAL, STATE AND FEDERAL REQUIREMENTS.
2. CULVERT PIPE SIZE AND NUMBER SHALL BE INCREASED TO ACCOMMODATE ANTICIPATED STREAM FLOW.
3. AGGREGATE FILL CROSSING SHOWN IN THE DETAIL. CONSTRUCTION MAT BRIDGE SHALL BE USED WHERE FEASIBLE.
4. INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
5. FOR MINOR WATERBODIES (< 10 FT. WIDE) TRENCHING AND BACKFILL IN THE WATERBODY SHALL BE COMPLETED WITHIN 24 CONTINUOUS HOURS AFTER INITIATING THE EXCAVATION. IF AUTHORIZED BY THE OSPC OR EPSC SPECIALIST, WORK IN INTERMEDIATE WATERBODIES (10 FT. TO 100 FT. WIDE) SHALL BE COMPLETED WITHIN 48 HOURS.

TYPICAL STREAM FLUME CROSSING

SCALE: N.T.S.

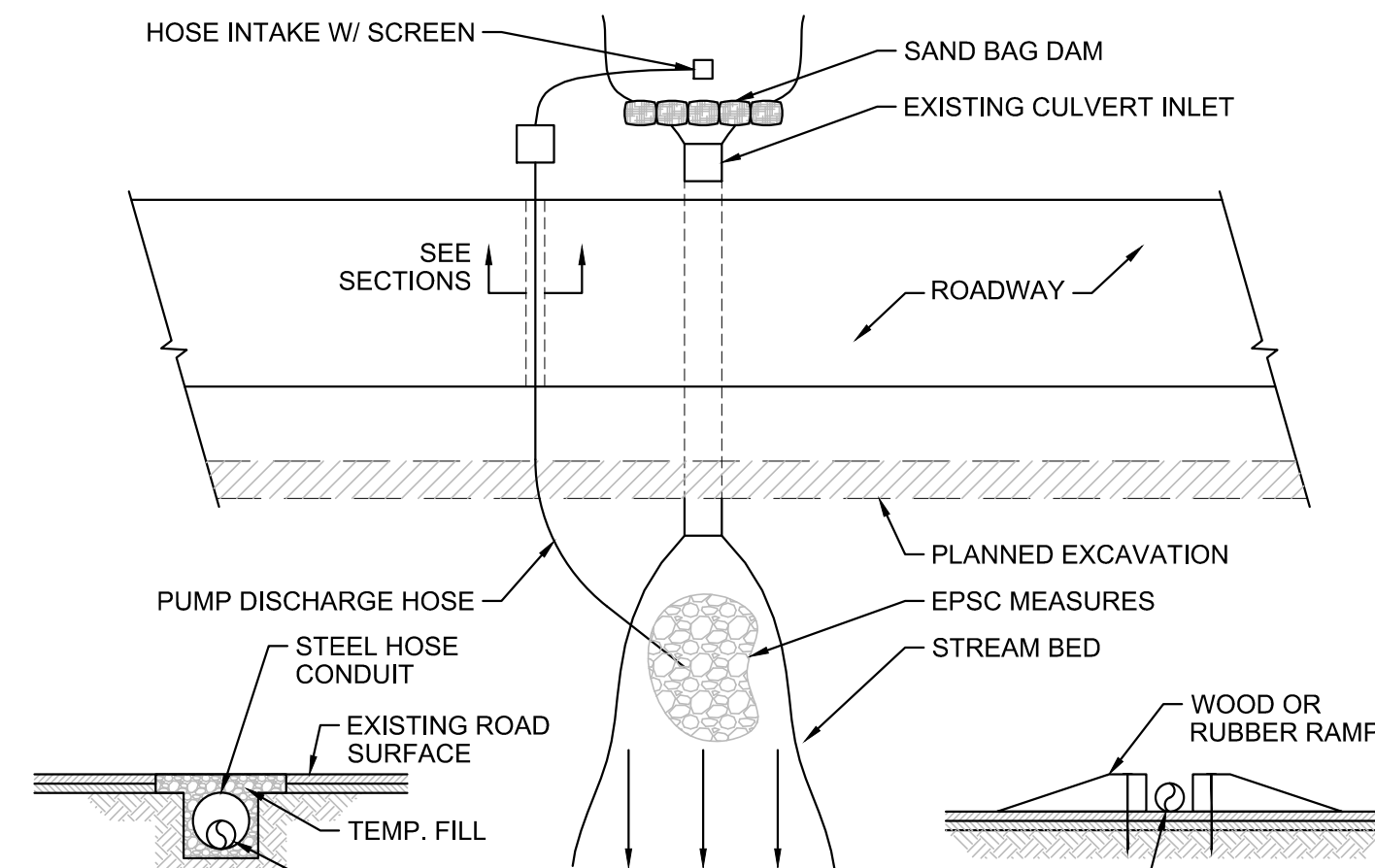


PLAN VIEW

SECTION A-A

NOTES

1. DIMENSIONS ARE CONCEPT ONLY AND SUBJECT TO MODIFICATION TO MEET MUNICIPAL, STATE AND FEDERAL REQUIREMENTS.
2. PUMP DISCHARGE HOSE, CULVERT PIPE SIZE AND NUMBER SHALL BE INCREASED TO ACCOMMODATE ANTICIPATED STREAM FLOW.
3. AGGREGATE FILL CROSSING SHOWN IN THE DETAIL. CONSTRUCTION MAT BRIDGE SHALL BE USED WHERE FEASIBLE.
4. INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
5. FOR MINOR WATERBODIES (< 10 FT. WIDE) TRENCHING AND BACKFILL IN THE WATERBODY SHALL BE COMPLETED WITHIN 24 CONTINUOUS HOURS AFTER INITIATING THE EXCAVATION WHERE FEASIBLE. IF AUTHORIZED BY THE OSPC OR EPSC SPECIALIST, WORK IN INTERMEDIATE WATERBODIES (10 FT. TO 100 FT. WIDE) SHALL BE COMPLETED WITHIN 48 HOURS.
6. UTILIZE INLET SCREEN ON INTAKE HOSE AND ELEVATE INTAKE ABOVE STREAMBED SEDIMENT TO THE EXTENT PRACTICABLE. PREVENT STREAMBED SCOUR AT PUMP DISCHARGE AND CONTINUOUSLY MONITOR DAM AND PUMPS TO ENSURE PROPER OPERATION THROUGHOUT THE CROSSING PROCEDURE.

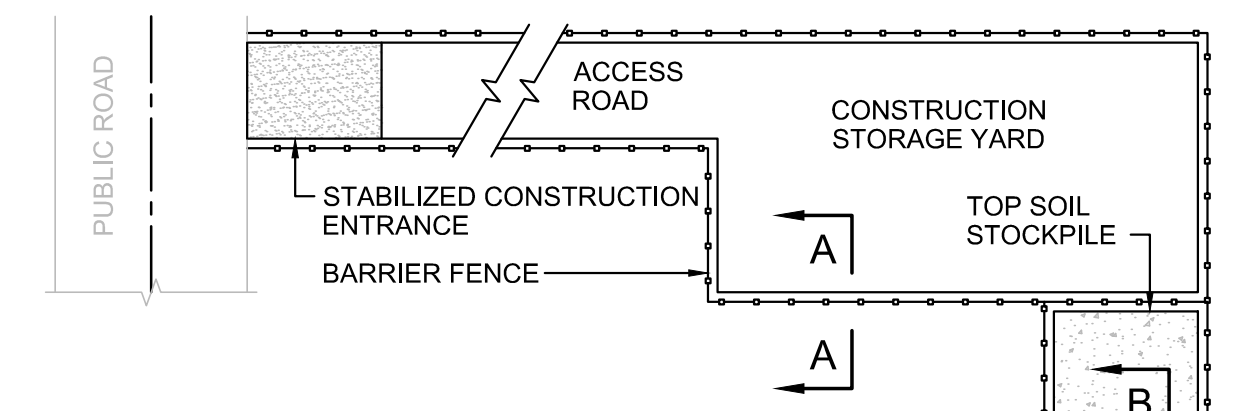


SECTION A-A

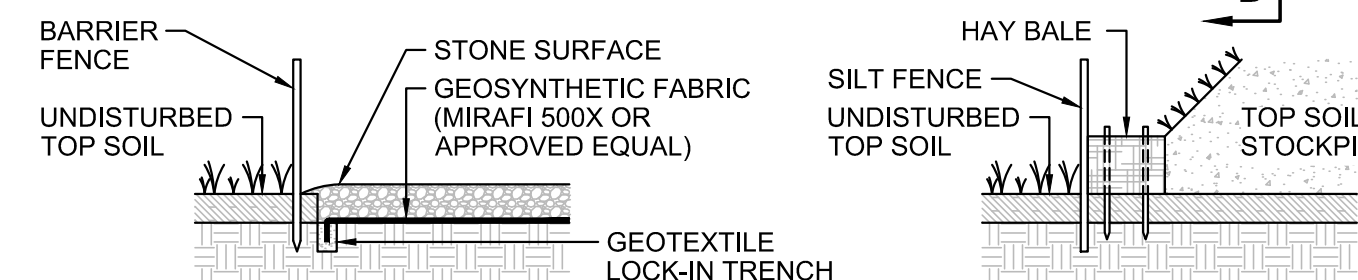
SECTION B-B

NOTES

1. AT CULVERT CABLE INSTALLATION WILL INCLUDE WATER FLOW CONTROL THROUGH THE AFFECTED CULVERT. STREAM FLOW MAY BE CONTROLLED USING ANY NUMBER OF APPROVED METHODS INCLUDING BY-PASS PUMPING, BY-PASS FLUME, OR TEMPORARY DAM. THE METHOD EMPLOYED WILL DEPEND ON THE STREAM FLOW RATE AND CLASSIFICATION. SPECIFIC METHOD AND SET-UP SHALL BE SUBJECT TO APPROVAL BY THE OSPC OR EPSC SPECIALIST.
2. PUMP DISCHARGE PIPE SHALL BE PROTECTED FROM DAMAGE BY TRAFFIC. PROTECTION METHOD SHALL PROVIDE TRAFFIC ACCESS THROUGH EMBEDMENT WITHIN ROADWAY (SECTION A-A), TRANSITION RAMP (SECTION B-B) OR OTHER APPROVED MEANS.
3. TRANSITION RAMP SHALL ONLY BE USED FOR SHORT DURATIONS (LESS THAN ONE DAY) IN LOW, SLOW TRAFFIC AREAS. FOR DISCHARGE HOSES OF 4 INCH DIAMETER OR LESS.
4. HOSE INTAKE SHALL INCLUDE APPROPRIATE SCREEN AND EPSC MEASURES TO LIMIT STREAM BOTTOM SCOUR/DISTURBANCE.
5. AT COMPLETION OF THE WORK ALL TEMPORARY MEASURES SHALL BE REMOVED AND THE SITE RESTORED TO PRE-CONSTRUCTION CONDITIONS.



CONSTRUCTION STORAGE YARD
(NOT TO SCALE)



SECTION A-A
(NOT TO SCALE)

SECTION B-B
(NOT TO SCALE)

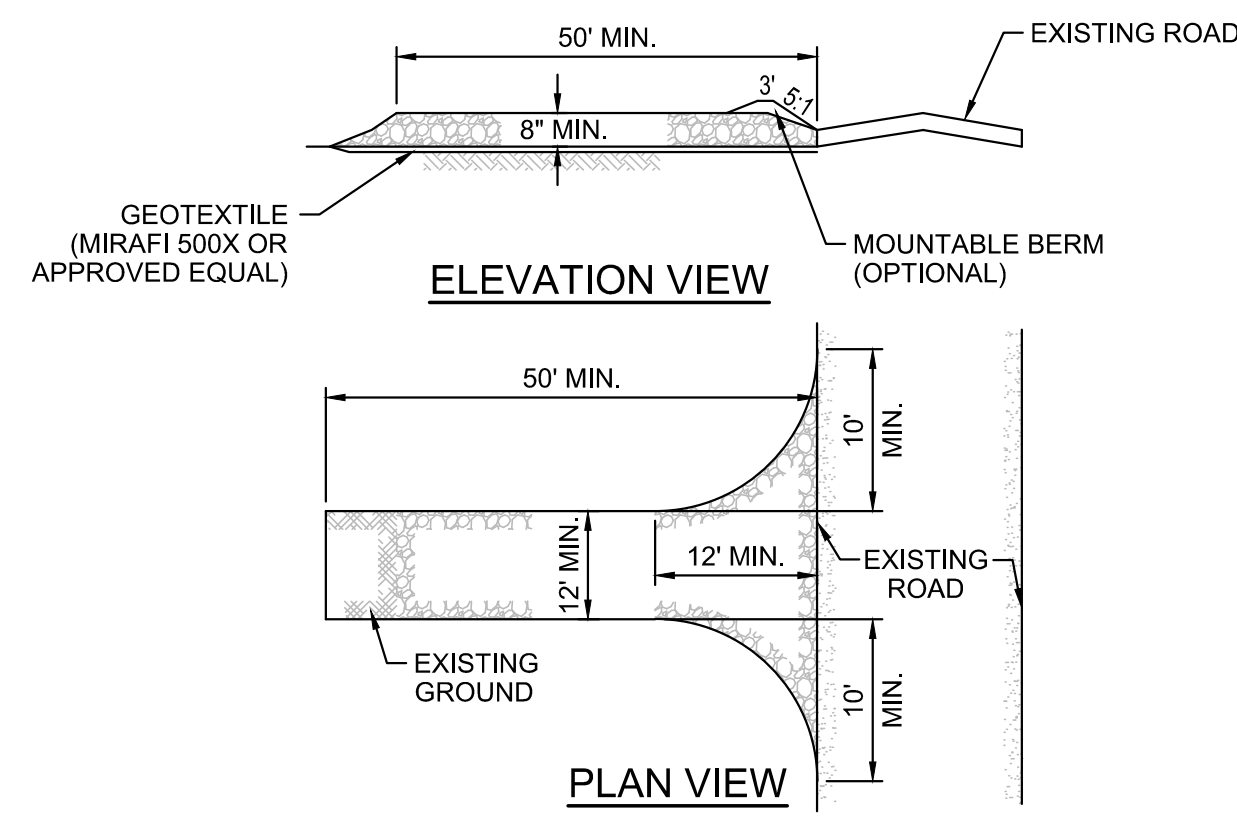
NOTES

1. CONSTRUCTION YARDS BUILT ON AGRICULTURAL LAND AND OTHER SENSITIVE SOILS SHALL BE STRIPPED OF TOP SOIL AND STOCKPILED FOR LATER RESTORATION OF AREA. STOCKPILED TOP SOIL SHALL BE PROTECTED BY SILT FENCE OR OTHER MEASURES TO LIMIT EROSION AND CONTROL SEDIMENT. STOCKPILES SHALL BE SEED AND MULCHED FOR LONG-TERM PROTECTION.
2. STORAGE YARD AND ACCESS ROAD SHALL BE BUILT UPON A GEOSYNTHETIC STABILIZATION/SEGREGATION FABRIC ANCHORED AT ITS EDGES USING A LOCK IN TRENCH OR SIMILAR MEANS. THE ACTIVE YARD WILL BE TOPPED WITH NOT LESS THAN 6 INCHES OF CRUSHED STONE OR GRAVEL. ENTIRE YARD PERIMETER AND ACCESS ROAD SHALL BE DEMARKED WITH ORANGE FENCE OR FLAGGING PER THE EPSC PLAN. PERIMETER CONTROLS (E.G. SILTFENCE) SHALL BE INSTALLED PER THE EPSC PLAN.
3. PUBLIC ROAD END OF ACCESS ROAD SHALL INCLUDE A STABILIZED CONSTRUCTION ENTRANCE. STABILIZED CONSTRUCTION ENTRANCE SHALL MEET PERMIT REQUIREMENTS AND VERMONT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
4. AT THE COMPLETION OF THE WORK, UNLESS OTHERWISE DIRECTED, THE CONSTRUCTION STORAGE YARD SHALL BE RESTORED TO ITS ORIGINAL CONDITION. RESTORATION SHALL INCLUDE LOOSENING THE TOP 6 IN. OF YARD SUBGRADE TO UNCOMPACT SOILS. SPREAD STOCKPILED TOP SOIL, SEED AND MULCH. ALL YARD GRAVEL, GEOSYNTHETICS, EPSC MATERIALS, WASTE AND DEBRIS SHALL BE REMOVED AND PROPERLY DISPOSED OF.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

No.	Revision	Date	By	Ck	PE	PE #
A	20% ANR Submission	12/5/14	TRC	AMW		
B	EPSC & PERMITS IFCR	3/6/15	TRC	AMW		
C	ISSUED FOR USE	3/27/15	TRC	AMW		
D	ISSUED FOR PERMITTING	7/24/15	TRC	AMW		

TDI New England
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TDI New England
Typical Details

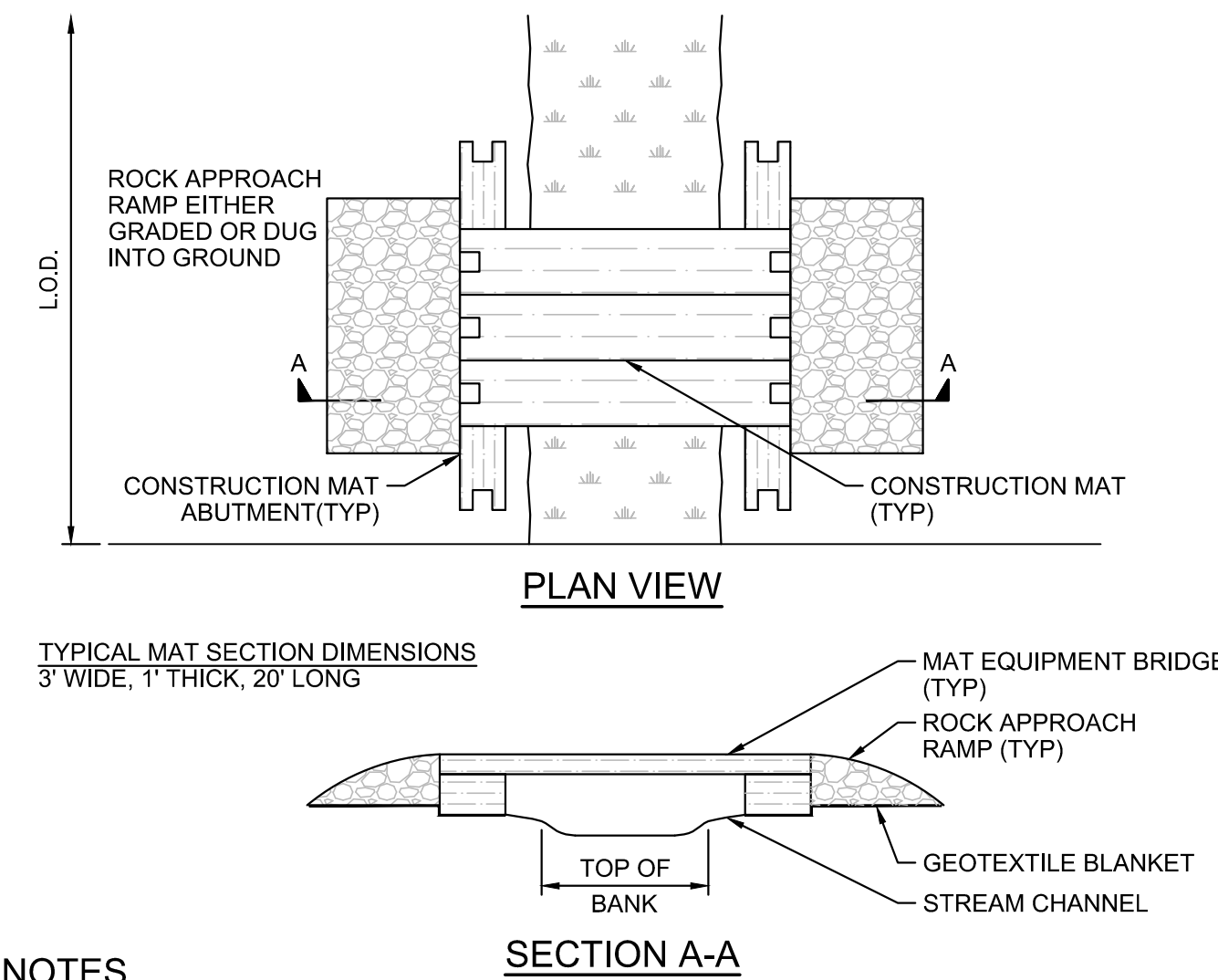


NOTES

1. STONE SIZE - USE 1"-4" STONE, RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
2. LENGTH - NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH APPLIES).
3. THICKNESS - NOT LESS THAN 8 INCHES.
4. WIDTH - 12-FOOT MIN. BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.
5. GEOTEXTILE - COVER ENTIRE AREA PRIOR TO PLACING STONE.
6. SURFACE WATER - ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CONSTRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5H:1V SLOPES SHALL BE USED IN LIEU OF A PIPE.
7. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY SHALL BE REMOVED IMMEDIATELY.
8. PERIODIC INSPECTION AND MAINTENANCE SHALL BE PROVIDED IN ACCORDANCE WITH PERMIT CONDITIONS.

STABILIZED CONSTRUCTION ENTRANCE

SCALE: N.T.S.

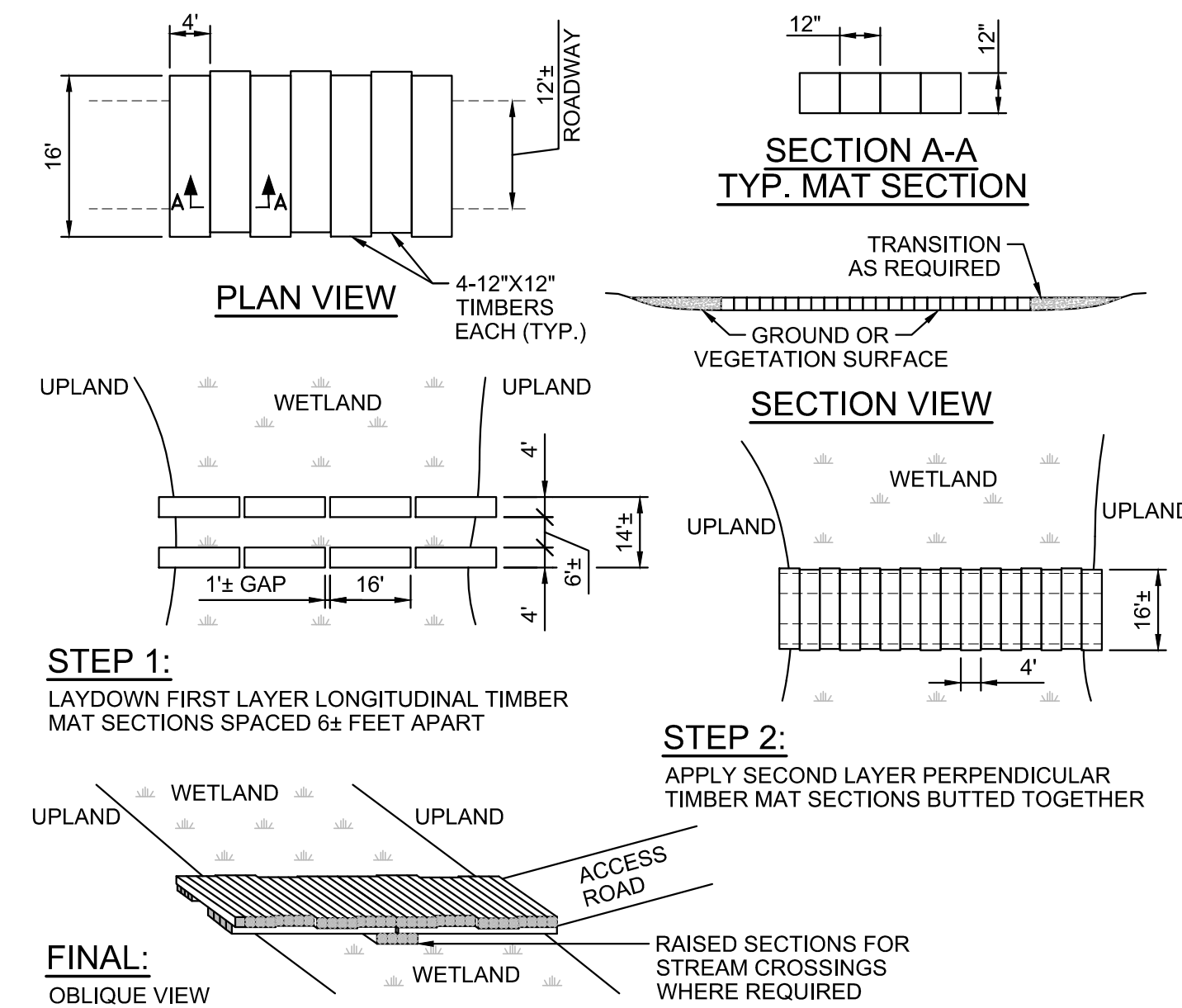


NOTES

1. ADDITIONAL MATS CAN BE PUT SIDE BY SIDE IF EXTRA WIDTH IS REQUIRED.
2. EQUIPMENT MATS SHALL ACCOMMODATE LARGEST EQUIPMENT USED. MATS MAY BE OF NATURAL HARDWOOD OR SUITABLE, APPROVED MAN-MADE MATERIALS.
3. ROCK APPROACH RAMP SHALL BE USED AT ENTRANCE TO THE EQUIPMENT BRIDGE. SEGREGATE IN-SITU SOIL FROM ROCK APPROACH RAMP USING GEOTEXTILE BLANKET.
4. INSTALL EPSC MEASURES IN ACCORDANCE WITH ISSUED PERMITS AND VT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL.
5. RESTORE RIPARIAN AREAS FOLLOWING REMOVAL OF TEMPORARY BRIDGE. REPAIR METHODS AND MATERIAL SHALL BE PER THE EPSC PLAN AND APPROVED PERMITS.
6. ADJUST CONSTRUCTION MAT LENGTH FOR WIDER STREAM CROSSINGS.

TEMPORARY EQUIPMENT BRIDGE

SCALE: N.T.S.

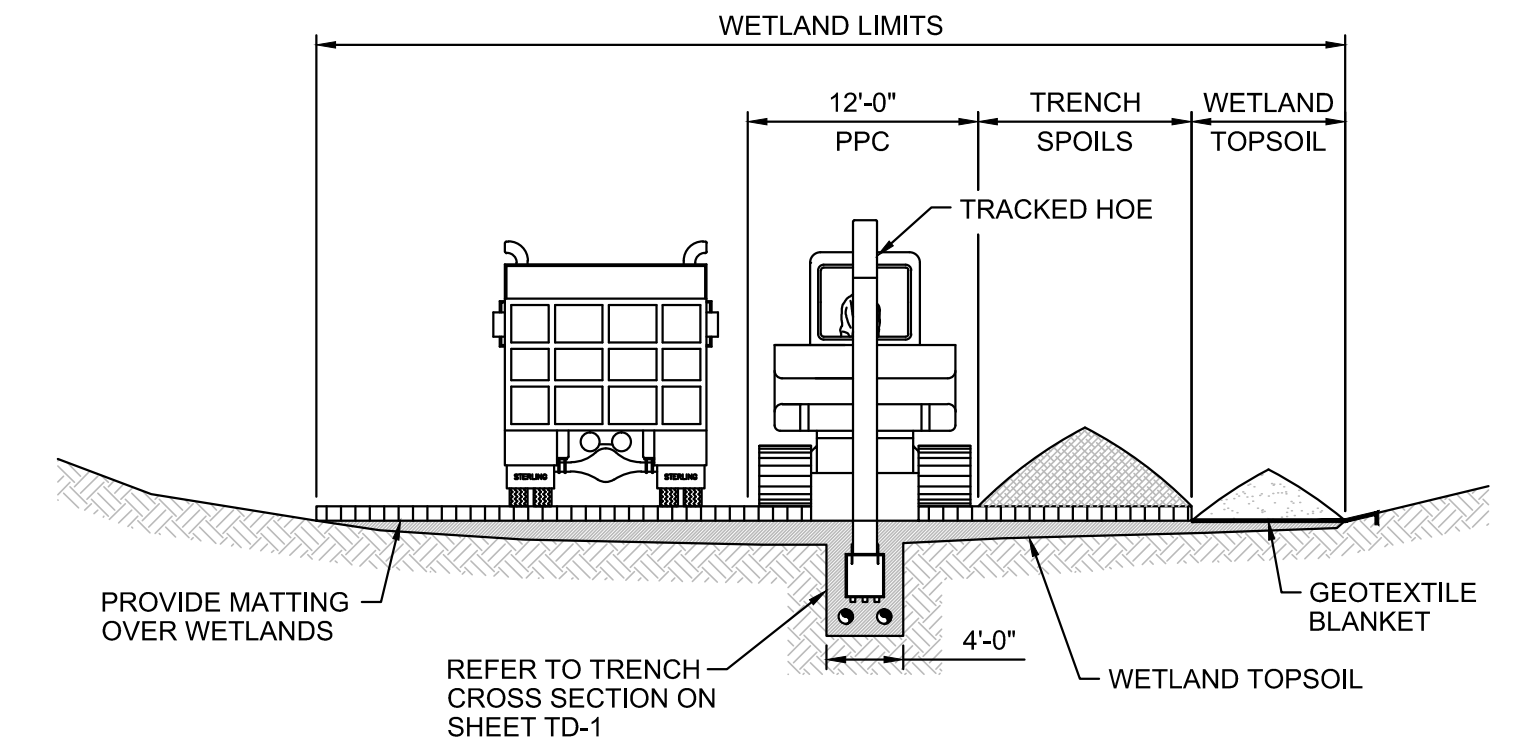


NOTES

1. TO BE INSTALLED WHERE NECESSARY IN WETLAND FOR ACCESS FOR CONSTRUCTION. ALTERNATIVE CONSTRUCTION MATTING (E.G., RUBBER MATS) MAY BE SUBSTITUTED FOR TIMBER MATTING.
2. PREPARATION FOR INSTALLATION OF TIMBER MATS WILL CONSIST OF CUTTING TALL WOODY SPECIES AND TRIMMING SHRUBS IF CONDITIONS REQUIRE. VEGETATION ROOT MASS IS TO REMAIN UNDISTURBED. MATS TO BE PLACED TO MAINTAIN NATURAL SOIL CONTOURS/CONDITIONS.
3. TIMBER SECTIONS TO BE SECURED TOGETHER WITH NO SPACES BY BOLTS, NAILS, STRAPS OR OTHER APPROPRIATE METHODS.
4. TIMBER MATS TO BE REMOVED UPON COMPLETION OF PROJECT AND AREA RESTORED TO NEAR ORIGINAL CONDITIONS PER EPSC PLANS.
5. SNOW/ICE REMOVAL BY MECHANICAL METHODS: NO DEICING SALT OR CHEMICALS TO BE USED. LIGHT APPLICATION OF SAND FOR TRACTION ACCEPTABLE SO AS RESIDUE DOES NOT ACCUMULATE IN WETLAND.
6. MATS ARE TO BE IN PLACE FOR MINIMUM DURATION FEASIBLE.

CONSTRUCTION MATTING - TIMBER MAT TYPICAL

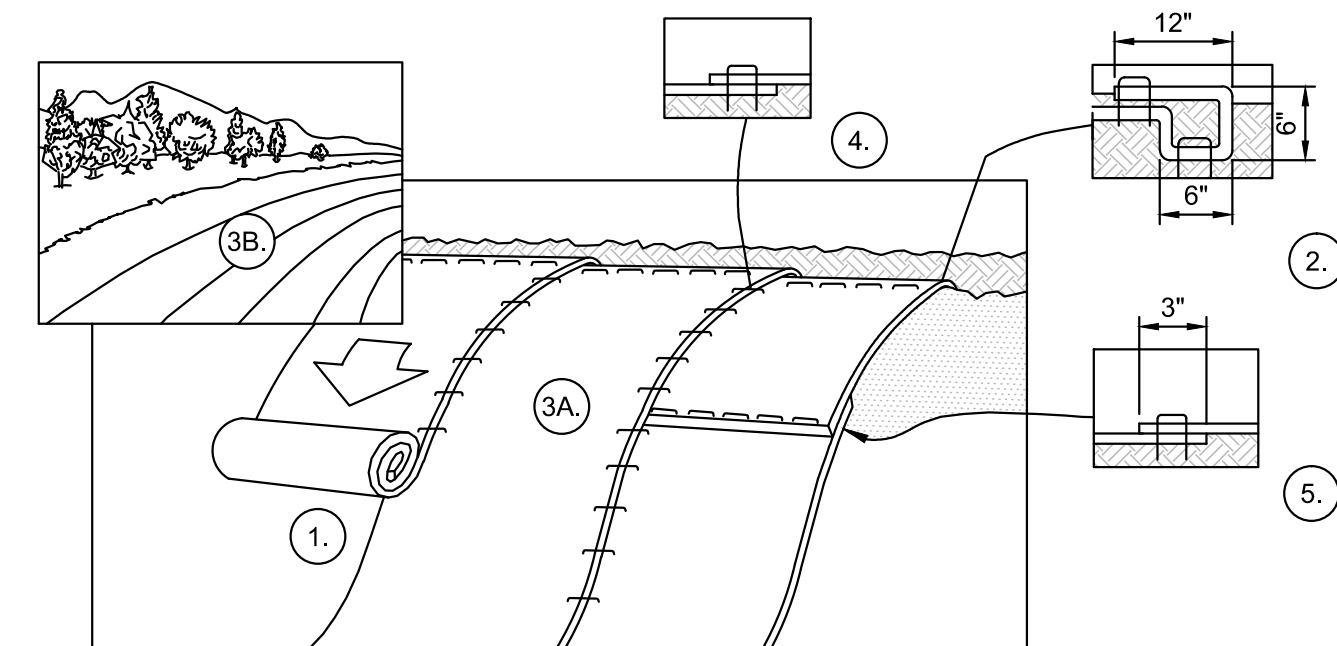
SCALE: N.T.S.



NOTES

1. EQUIPMENT ACCESS SHALL BE UNDER DRY OR FROZEN CONDITIONS, OR BY USE OF CONSTRUCTION MATS OR AS SPECIFIED IN WETLAND PERMITS.
2. PROVIDE TEMPORARY TRENCH BREAKER AT EACH EDGE OF STREAM AND WETLAND EXCAVATION.
3. TOPSOIL AND TRENCH SPOILS SHALL BE SEGREGATED AND STOCKPILED ON CONSTRUCTION MATS OR GEOTEXTILE FABRIC WITHIN WETLAND AREAS.
4. TRENCH SHALL BE BACKFILLED WITH SOILS PLACED IN REVERSE ORDER OF HOW THEY WERE REMOVED. UPPER LAYER FILL SHALL BE WETLAND TOPSOIL PLACED TO A DEPTH EQUAL TO THAT OF THE ADJACENT IN-SITU NATIVE TOPSOIL.
5. AT COMPLETION OF THE WORK REMOVE GEOTEXTILE AND CONSTRUCTION MATTING. CONSTRUCTION MATS SHALL BE THOROUGHLY CLEANED IN ACCORDANCE WITH THE EPSC PLAN AND PROJECT PERMITS PRIOR TO USE AT OTHER LOCATIONS.
6. IMPLEMENT EPSC MEASURES IN ACCORDANCE WITH THE EPSC PLAN.

TYPICAL WETLAND CONSTRUCTION



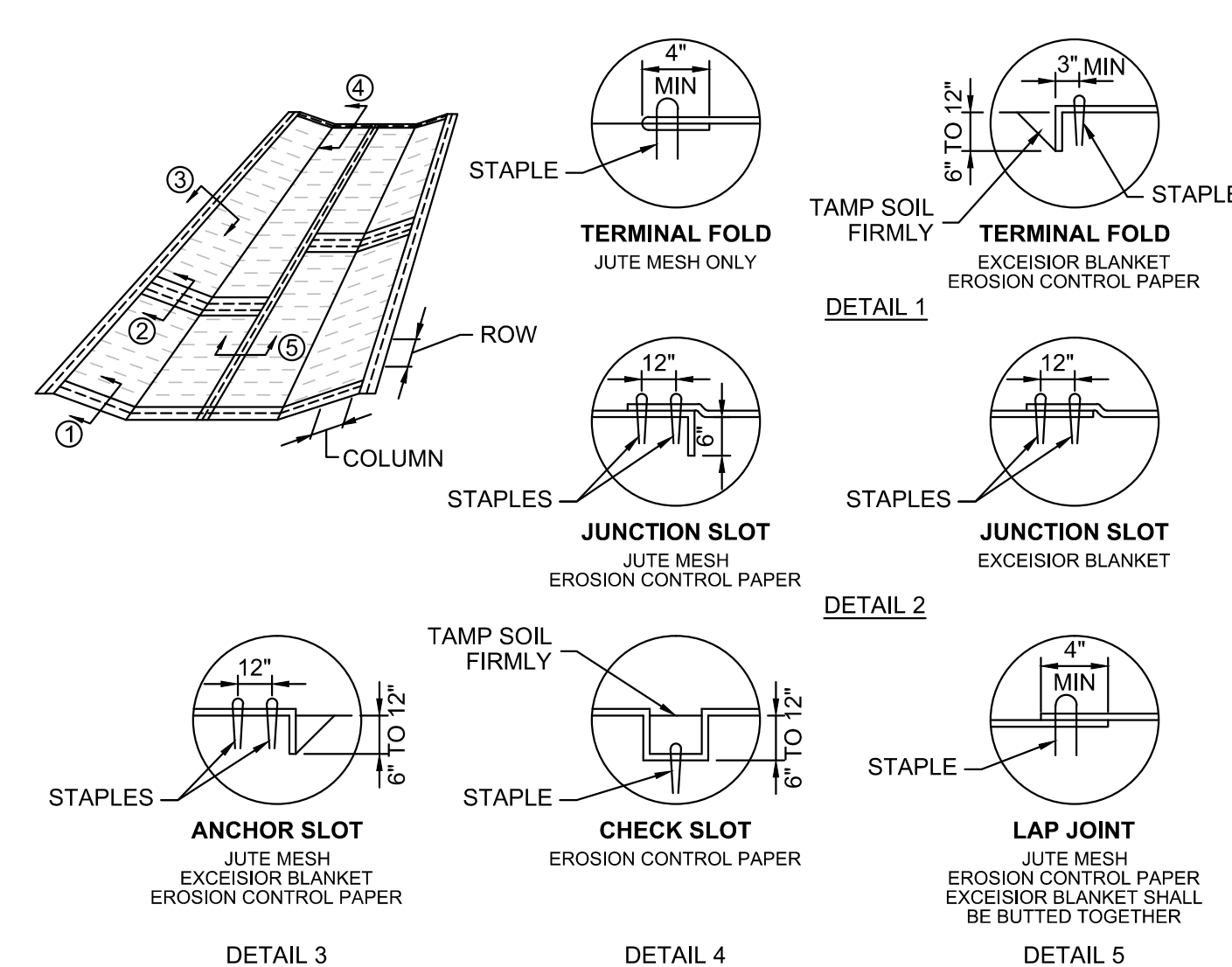
1. PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING ANY NECESSARY APPLICATION OF LIME, FERTILIZER, AND SEED. NOTE: WHEN USING CELL-O-SEED DO NOT SEED PREPARED AREA. CELL-O-SEED MUST BE INSTALLED WITH PAPER SIDE DOWN.
2. BEGIN AT THE TOP OF THE SLOPE BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH WITH APPROXIMATELY 12" OF BLANKET EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR THE BLANKET WITH A ROW OF STAPLES/STAKES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF BLANKET BACK OVER SEED AND COMPACTED SOIL. SECURE BLANKET OVER COMPACTED SOIL WITH A ROW OF STAPLES/STAKES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE BLANKET.
3. ROLL THE BLANKETS (A) DOWN OR (B) HORIZONTALLY ACROSS THE SLOPE. BLANKETS WILL UNROLL WITH APPROPRIATE SIDE AGAINST THE SOIL SURFACE. ALL BLANKETS MUST BE SECURELY FASTENED TO SOIL SURFACE BY PLACING STAPLES/STAKES IN APPROPRIATE LOCATIONS AS SHOWN IN THE STAPLE PATTERN GUIDE. WHEN USING OPTIONAL DOT SYSTEM, STAPLES/STAKES SHOULD BE PLACED THROUGH EACH OF THE COLORED DOTS CORRESPONDING TO THE APPROPRIATE STAPLE PATTERN.
4. THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH APPROXIMATELY 2"-5" OVERLAP DEPENDING ON BLANKET TYPE. TO ENSURE PROPER SEAM ALIGNMENT, PLACE THE EDGE OF THE OVERLAPPING BLANKET (BLANKET BEING INSTALLED ON TOP) EVEN WITH THE COLORED SEAM STITCH ON THE PREVIOUSLY INSTALLED BLANKET.
5. CONSECUTIVE BLANKETS SPICED DOWN THE SLOPE MUST BE PLACED END OVER END (SHINGLE STYLE) WITH AN APPROXIMATE 3" OVERLAP. STAPLE THROUGH OVERLAPPED AREA, APPROXIMATELY 12" APART ACROSS ENTIRE BLANKET WIDTH.

NOTES

- RECP SHALL BE COMPRISED OF BIODEGRADABLE NATURAL FIBER NETTING/MESH MATERIAL AS OPPOSED TO MATERIAL THAT CONTAINS CHEMICALLY-BOUND JOINTS IN THE SUPPORTING MESH THAT COULD BE HARMFUL TO WILDLIFE. ONLY PROCESSED BIODEGRADABLE NATURAL FIBERS MECHANICALLY BOUND TOGETHER BETWEEN A SINGLE (IF SINGLE NET) OR A DOUBLE (IF DOUBLE NET) LAYER OF NATURAL FIBER NETTING OF PROCESSED YARNS OR TWINES THAT ARE WOVEN OR INTERLINKED ARE ALLOWED.
- IN LOOSE SOIL CONDITIONS, THE USE OF STAPLE OR STAKE LENGTHS GREATER THAN 6 INCHES MAY BE NECESSARY TO PROPERLY SECURE THE BLANKETS.

ROLLED EROSION CONTROL PRODUCT (RECP) - SLOPE INSTALLATION

SCALE: N.T.S.

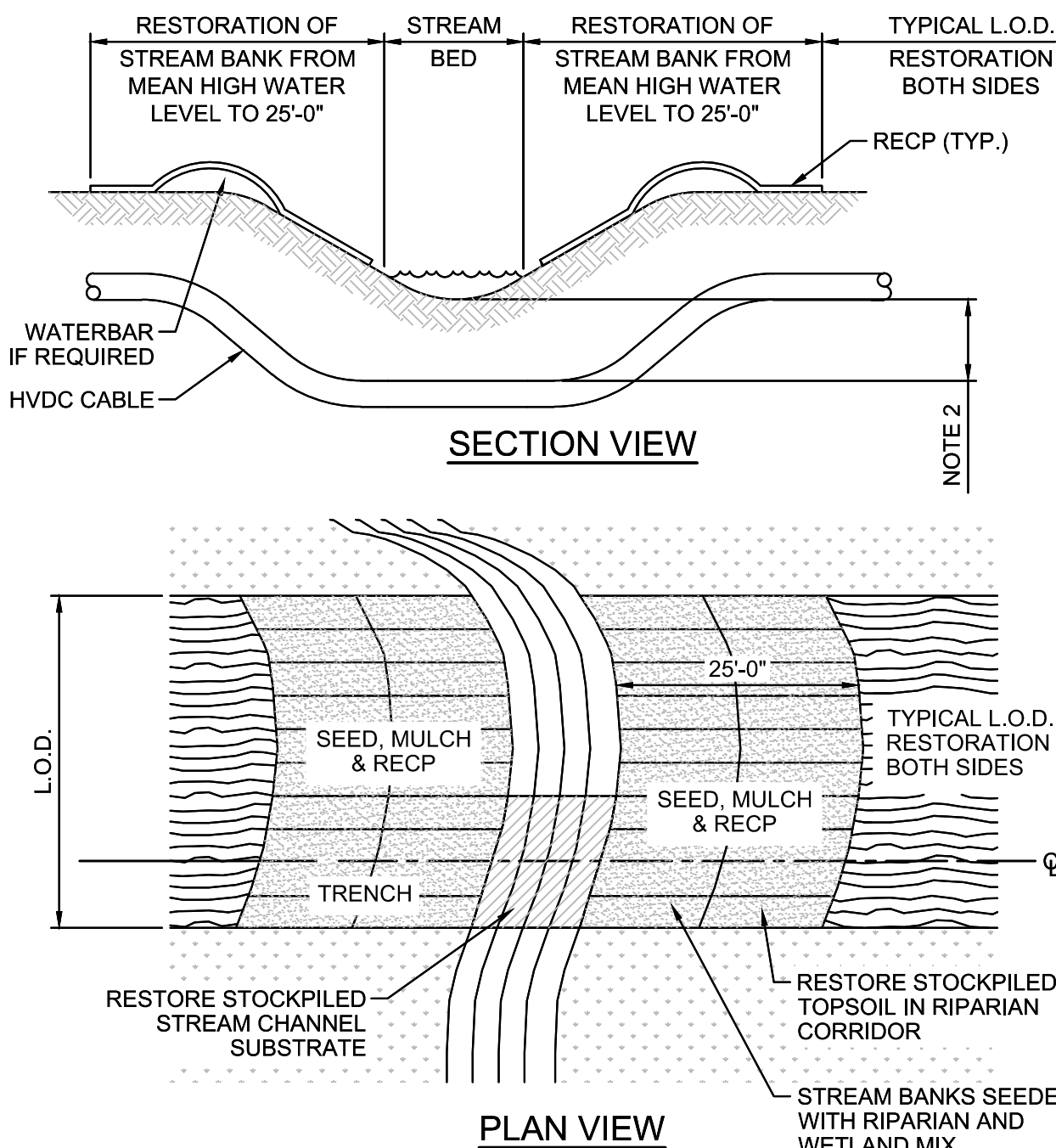


NOTES

1. INSTALL ROLLED EROSION CONTROL PRODUCT (RECP) EVERY 50' WHERE 4% < SLOPE < 6% ON SLOPES OF 6% OR MORE SPACE AT 25' INTERVALS.
2. STAPLES SHALL BE PLACED ALTERNATELY, IN COLUMNS APPROXIMATELY 2' APART AND IN ROWS APPROXIMATELY 3' APART.
3. DISTURBED AREA SHALL BE GRADED SMOOTH WITH CLOSE CONTACT BETWEEN RECP AND GROUND.
4. PLACE EROSION CONTROL MATERIAL LOOSLEY WITHOUT STRETCHING.
5. ALL TERMINAL ENDS AND TRANSVERSE LAPS SHALL BE STAPLED AT APPROXIMATELY 12 INCH INTERVALS.
6. STAPLES SHALL BE A MINIMUM OF 1 INCH WIDE WITH LEGS OF 6 TO 12 INCHES LONG.
7. RECP SHALL BE COMPRISED OF BIODEGRADABLE NATURAL FIBER NETTING/MESH MATERIAL AS OPPOSED TO MATERIAL THAT CONTAINS CHEMICALLY-BOUND JOINTS IN THE SUPPORTING MESH THAT COULD BE HARMFUL TO WILDLIFE. ONLY PROCESSED BIODEGRADABLE NATURAL FIBERS MECHANICALLY BOUND TOGETHER BETWEEN A SINGLE (IF SINGLE NET) OR A DOUBLE (IF DOUBLE NET) LAYER OF NATURAL FIBER NETTING OF PROCESSED YARNS OR TWINES THAT ARE WOVEN OR INTERLINKED ARE ALLOWED.

ROLLED EROSION CONTROL PRODUCT (RECP)

SCALE: N.T.S.



NOTES

1. STREAM CROSSING SHALL BE CONDUCTED IN ACCORDANCE WITH ESTABLISHED EPSC PLANS, APPROVED PROJECT PERMITS AND AS DIRECTED BY THE ON-SITE ENVIRONMENTAL SPECIALIST.
2. CABLE DEPTH UNDER STREAM SHALL BE IN ACCORDANCE WITH THE EPSC PLAN REQUIREMENTS.

STREAM BANK RESTORATION WITH RECP

SCALE: N.T.S.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

No.	Revision	Date	By	Ck	PE	PE #
A	20% ANR Submission	12/5/14	TRC	AMW		
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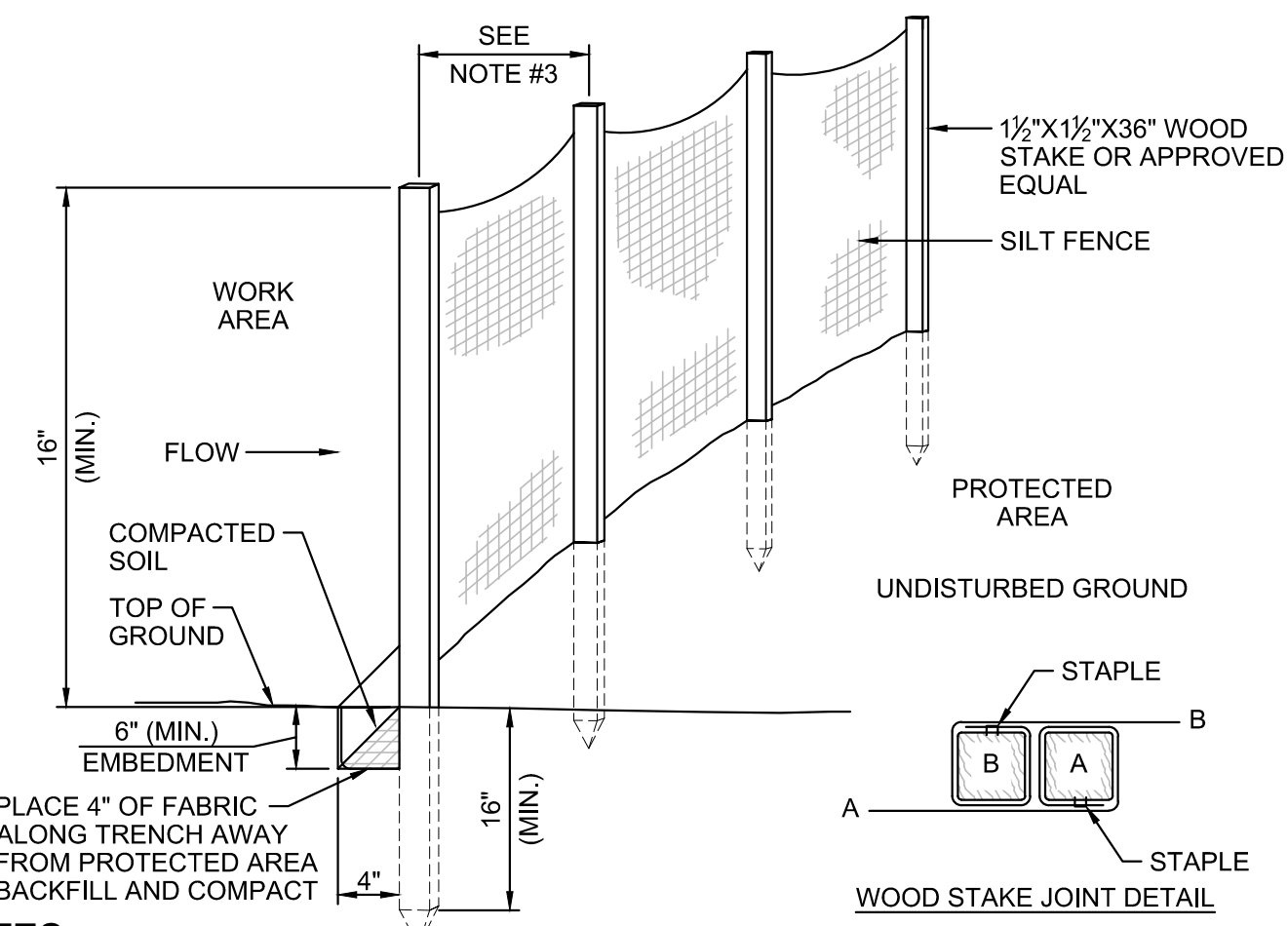
New England Clean Power Link

 TDI New England

 Typical Details

 TD-7

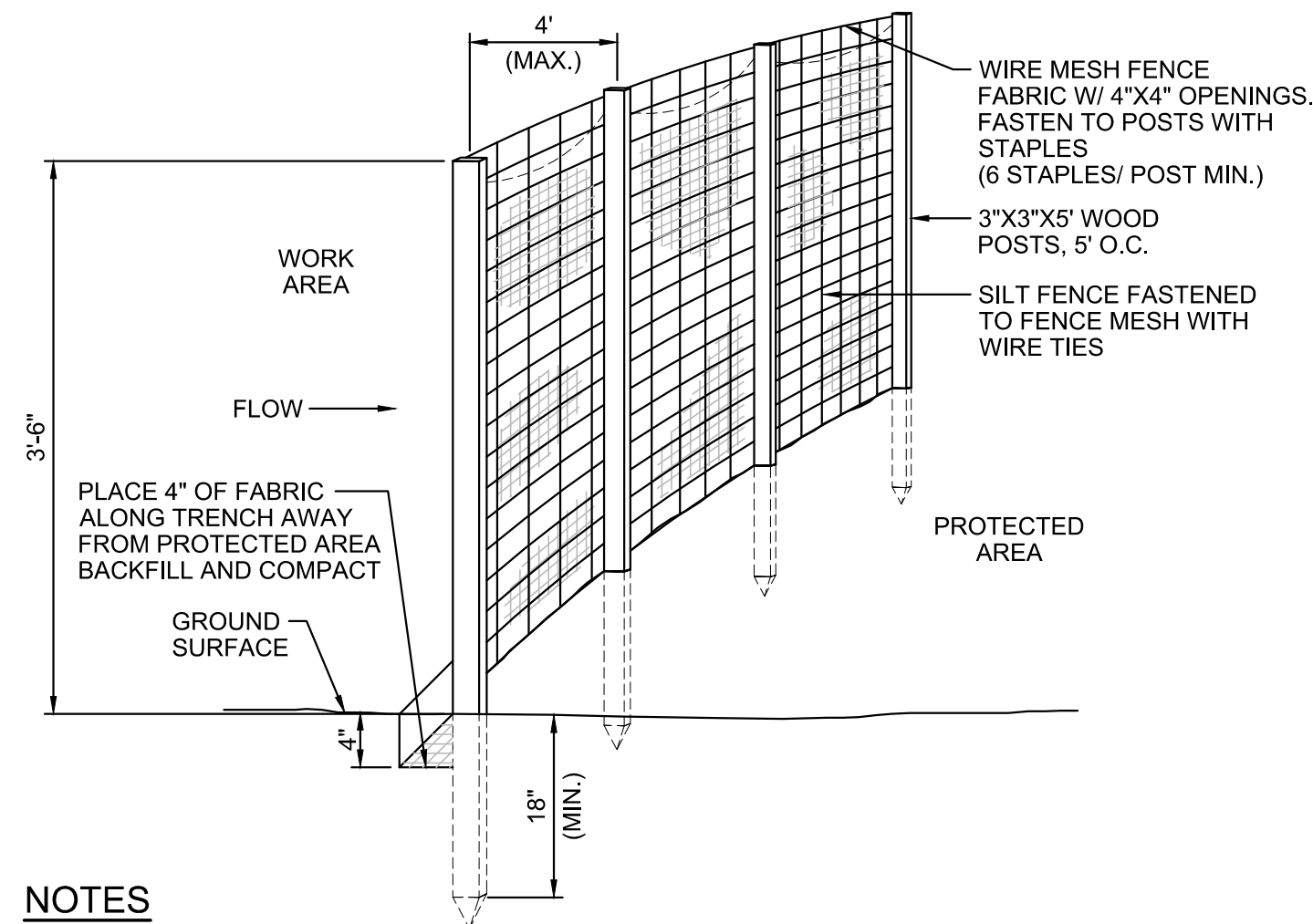
 Prepared by: TRC 10/09/14



NOTES

1. FILTER CLOTH SHALL BE EITHER FILER X, MIRAFI 100X, STABLINKA T140N OR APPROVED EQUIVALENT. MANUFACTURED SILT FENCE SHALL CONFORM TO THE MOST CURRENT VERMONT DEPARTMENT OF TRANSPORTATION (VTTRANS OR VAOI) STANDARDS.
2. FOR FILTER CLOTH FENCE WHEN ELONGATION IS >50%, POST SPACING SHALL NOT EXCEED 4 FEET. FOR FILTER CLOTH FENCE WHEN ELONGATION IS <50%, POST SPACING SHALL NOT EXCEED 6 FEET.
3. PREFABRICATED UNITS SHALL BE GEOFAB, ENVIROFENCE OR APPROVED EQUIVALENT.
4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN SEDIMENT REACHES HALF OF FABRIC HEIGHT. DISPOSE OF ACCUMULATED SOIL IN AN UPLAND AREA.
5. PERIMETER CONTROLS SHALL NOT CROSS ACTIVE ROUTES (E.G., ROADS) OR ACTIVE FLOW PATHS (E.G., LARGER STREAMS OR RIVERS).
6. PERIMETER CONTROLS SHALL REMAIN IN PLACE AND BE MAINTAINED/REPLACED AS NEEDED UNTIL FINAL STABILIZATION IN THE AREA HAS BEEN ACHIEVED.
7. WITHIN 50 FEET OF OPEN WATER, PERIMETER CONTROLS SHALL INCLUDE REINFORCED SILT FENCE.

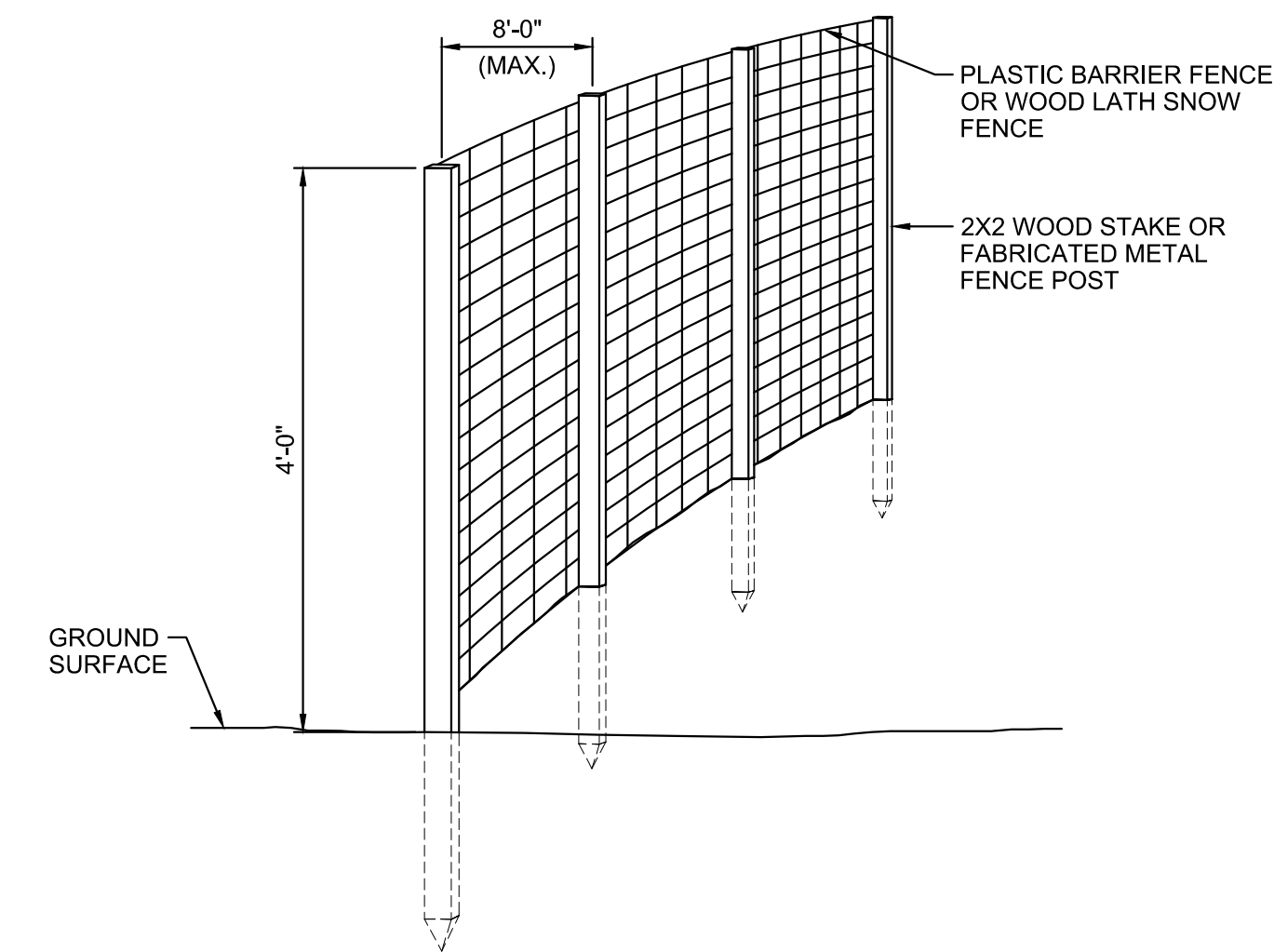
SILT FENCE
SCALE: N.T.S.



NOTES

1. FILTER CLOTH SHALL BE EITHER FILER X, MIRAFI 100X, STABLINKA T140N OR APPROVED EQUIVALENT. MANUFACTURED SILT FENCE SHALL CONFORM TO THE MOST CURRENT VERMONT DEPARTMENT OF TRANSPORTATION (VTTRANS OR VAOI) STANDARDS.
2. FOR FILTER CLOTH FENCE WHEN ELONGATION IS >50%, POST SPACING SHALL NOT EXCEED 4 FEET. FOR FILTER CLOTH FENCE WHEN ELONGATION IS <50%, POST SPACING SHALL NOT EXCEED 6 FEET.
3. WHEN TWO SECTIONS OF FILTER CLOTH ADJOIN EACH OTHER THEY SHALL BE OVER-LAPPED BY 6 INCHES AND FOLDED.
4. FILTER CLOTH SHALL BE FASTENED ON THE UPSTREAM FACE OF THE REINFORCING WIRE MESH.
5. PREFABRICATED UNITS SHALL BE GEOFAB, ENVIROFENCE OR APPROVED EQUIVALENT.
6. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN SEDIMENT REACHES HALF OF FABRIC HEIGHT AND DISPOSED OF IN AN UPLAND AREA.
7. PERIMETER CONTROLS SHALL NOT CROSS ACTIVE ROUTES (E.G., ROADS) OR ACTIVE FLOW PATHS (E.G., LARGER STREAMS OR RIVERS).
8. PERIMETER CONTROLS SHALL REMAIN IN PLACE AND BE MAINTAINED/REPLACED AS NEEDED UNTIL FINAL STABILIZATION IN THE AREA HAS BEEN ACHIEVED.

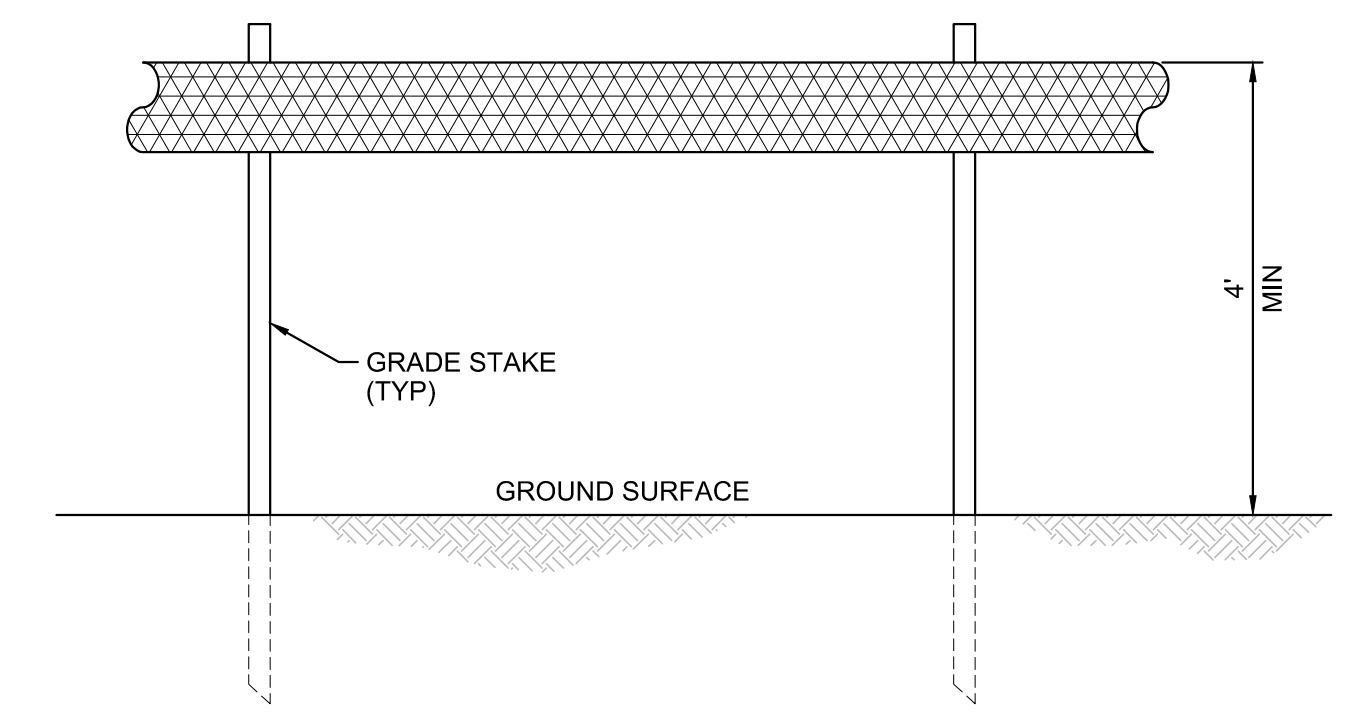
REINFORCED SILT FENCE WITH WIRE MESH
SCALE: N.T.S.



NOTES

1. CONSTRUCTION FENCE SHALL BE ORANGE PLASTIC BARRIER FENCE, WOOD LATH SNOW FENCE OR APPROVED ALTERNATIVE.
2. PLASTIC FENCING MATERIAL SHALL BE 100% RECYCLABLE AND MANUFACTURED FROM POST CONSUMER PRODUCTS (TENAX OR APPROVED EQUIVALENT).
3. SUPPORT FENCING ON 2X2 WOOD STAKES OR FABRICATED METAL FENCE POSTS. POST LENGTH SHALL BE AT LEAST 5'-6" LONG WITH 1'-6" EMBEDDED IN THE GROUND.
4. PERFORM MAINTENANCE OF BARRIER FENCE AS REQUIRED. AT THE COMPLETION OF THE WORK REMOVE THE FENCE AND RESTORE THE SITE TO PRE-CONSTRUCTION CONDITIONS.

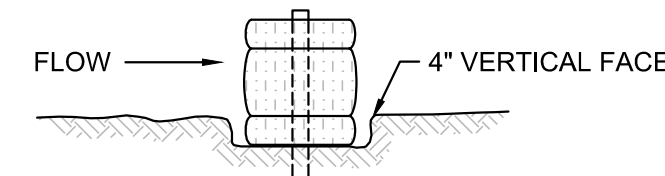
CONSTRUCTION FENCE
SCALE: N.T.S.



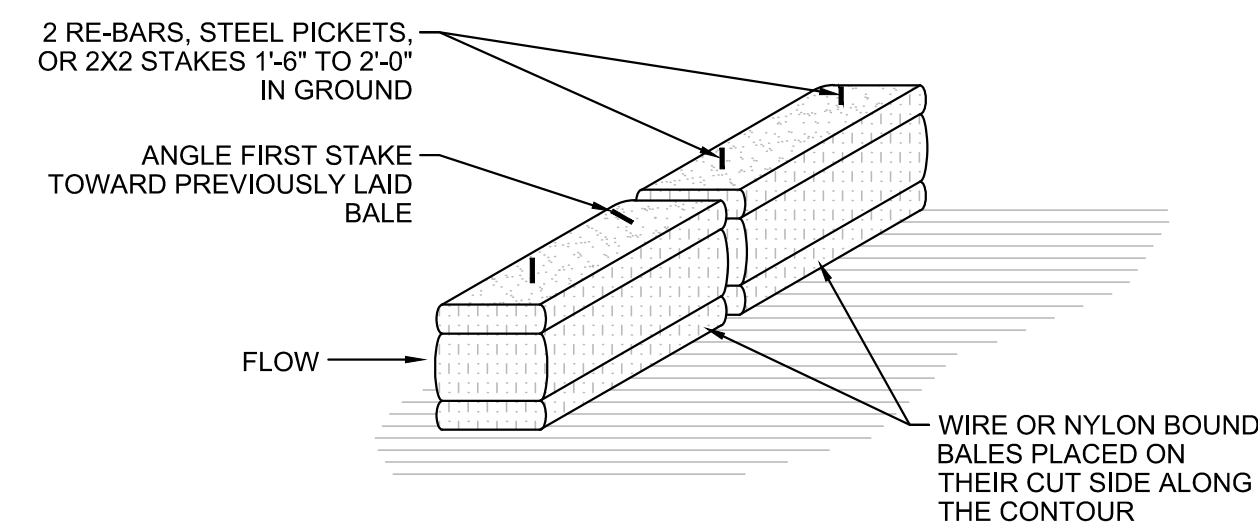
NOTES

1. CONSTRUCTION DEMARCATION SHALL BE INSTALLED ALONG THE PERIMETER OF THE LIMITS OF DISTURBANCE.
2. CONSTRUCTION DEMARCATION SHALL NOT CROSS ACTIVE ACCESS ROUTES.
3. IN AREAS MORE THAN 50 FEET FROM MAPPED WATER RESOURCES, CONSTRUCTION DEMARCATION MAY BE ORANGE FLAGGING TIED TO STAKES OR TREES, BARRIER MESH TAPE, 1/2 INCH YELLOW POLYPROPYLENE ROPE OR OTHER APPROVED METHODS.
4. WITHIN 50 FEET OF MAPPED WATER RESOURCES THE CONSTRUCTION DEMARCATION MAY BE: ORANGE BARRIER MESH TAPE, ROPE, ORANGE CONSTRUCTION FENCE, SNOW FENCE OR OTHER APPROVED DEMARCATION METHODS.
5. BARRIER TAPE AND FENCING SHALL BE INSTALLED BY FASTENING TO STAKES OR POSTS DRIVEN INTO THE GROUND. STAKE/POST DIMENSIONS AND SPACING SHALL BE SUFFICIENT TO SUPPORT DEMARCATION MATERIAL IN ALL ANTICIPATED WEATHER AND ENVIRONMENTAL CONDITIONS ANTICIPATED AT THE SITE.
6. CONSTRUCTION DEMARCATION SHALL BE MAINTAINED AND REMAIN IN-PLACE THROUGH THE COMPLETION OF THE WORK AND FINAL STABILIZATION OF THE AREAS.

BARRIER MESH TAPE OR ROPE
SCALE: N.T.S.



EMBEDDING DETAIL

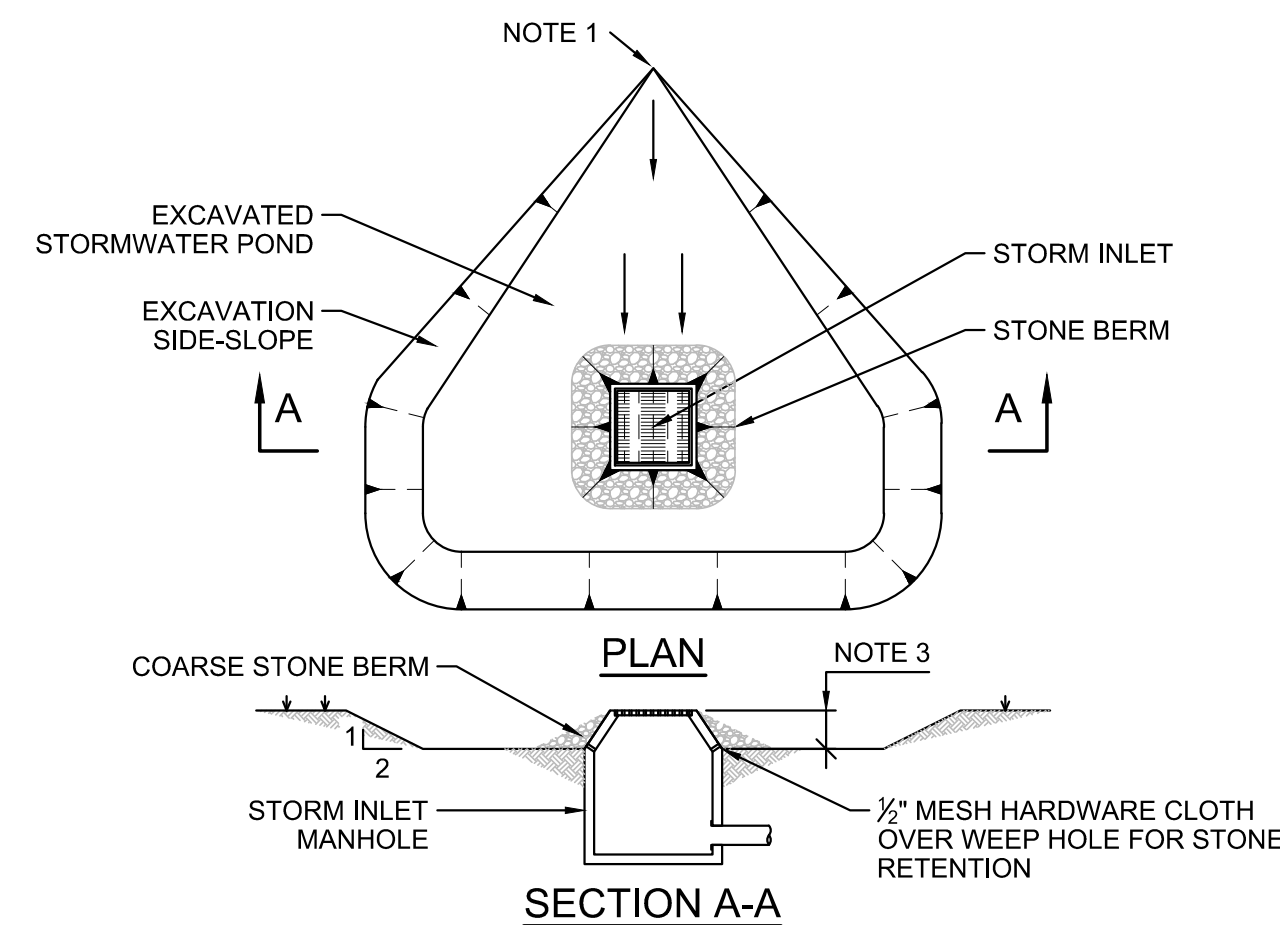


ANCHORING DETAIL

NOTES

1. REINFORCE SILT FENCE USING STRAW BALE DIKES AT STOCKPILES AND WHERE DIRECTED. STRAW BALES SHALL NOT BE USED AS A STAND-ALONE PRACTICE.
2. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN SEDIMENT REACHES 1/2 THE HEIGHT OF THE DIKE.
3. EROSION CONTROL MEASURES SHALL BE REMOVED AT THE COMPLETION OF THE WORK AND SURFACES RESTORED TO THEIR ORIGINAL CONDITION UNLESS OTHERWISE DIRECTED.

STRAW BALE DIKE
SCALE: N.T.S.

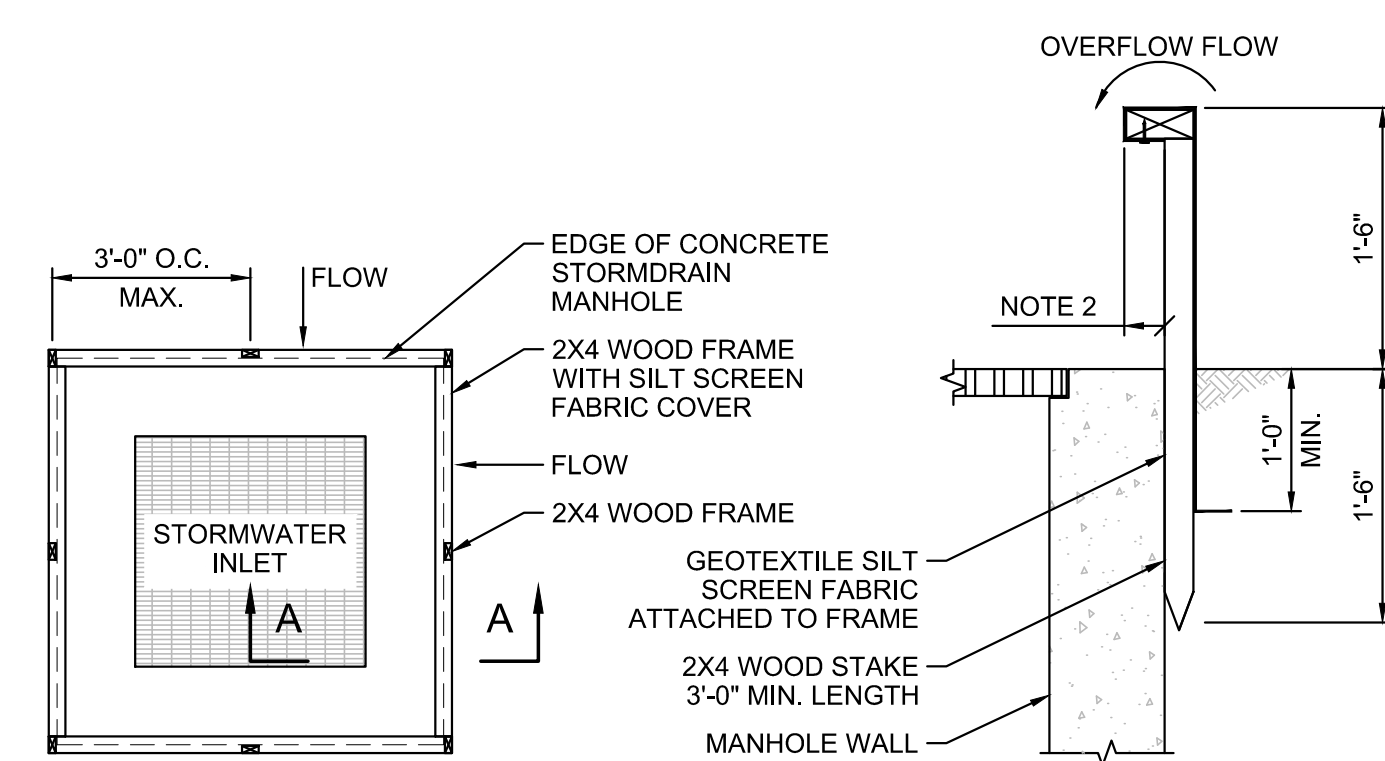


EXCAVATED INLET PROTECTION

NOTES

1. SHAPE INLET EXCAVATION TO FIT CONSTRUCTION SITE. ORIENT EXCAVATION WITH LONGEST SIDE IN DIRECTION OF HIGHEST ANTICIPATED FLOW.
2. EXCAVATED POND TRIBUTARY AREA SHALL BE LIMITED TO ONE ACRE OR LESS.
3. POND DEPTH SHALL BE NOT LESS THAN 1'-0" NOR GREATER THAN 2'-0". DESIGN FOR A CAPACITY OF 900 CUBIC FEET PER ACRE.
4. BASIN STORMWATER PROTECTION SHALL BE EMPLOYED IN CONJUNCTION WITH OTHER EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH EPSC PLAN.
5. PROVIDE MIN. 4 EACH, 2 INCH DIAMETER WEEP HOLES FOR STORMWATER POND DRAINAGE. NUMBER OF WEEP HOLES SHALL BE FIELD DETERMINED.
6. STORMWATER POND EXCAVATED SIDE-SLOPE SHALL BE GRADED AT A MAXIMUM SLOPE OF 2 H:1 V PROVIDE SOIL STABILIZATION IN ACCORDANCE WITH EPSC PLAN.
7. UPON STABILIZATION OF THE TRIBUTARY AREA, PLUG WEEP HOLES, PROPERLY FILL BASIN EXCAVATION AND STABILIZE THE SOIL PER THE EPSC REQUIREMENTS.

STORMWATER INLET PROTECTION
SCALE: N.T.S.



PLAN

SECTION A-A

FABRIC INLET PROTECTION


NOTES

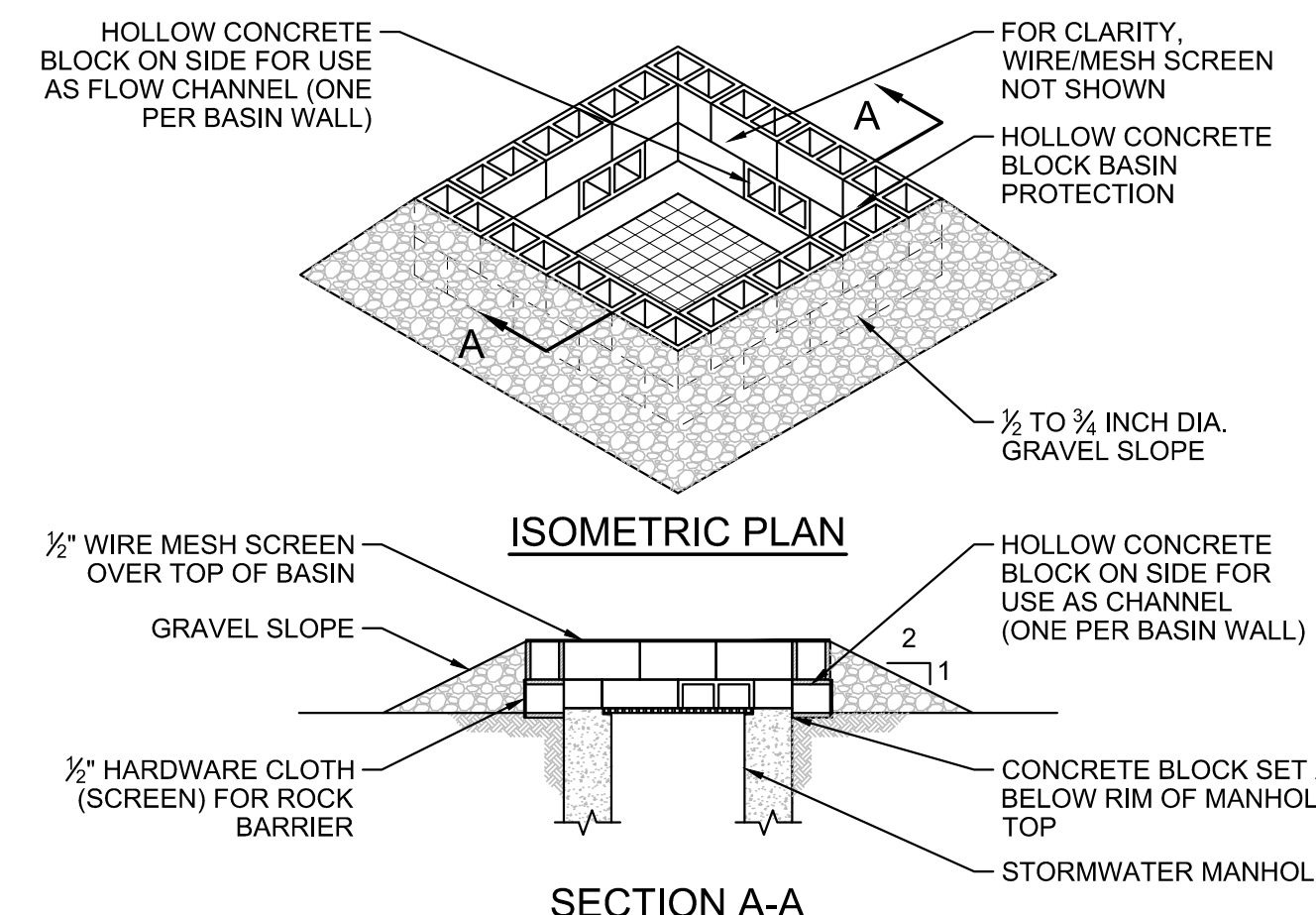
1. STORM INLET PROTECTION SHALL BE EMPLOYED WITH OTHER EPSC MEASURES IN ACCORDANCE WITH THE EPSC PLAN.
2. STORMWATER INLET SHALL BE CONSTRUCTED TO ENSURE OVERFLOW WATER DROP TO INLET GRATE OR CONCRETE.
3. WOOD STAKES SHALL BE MIN. 3'-0" LONG @ 3'-0" MAX. O.C., SPACED EVENLY AROUND PERIMETER. DRIVE STAKES NOT LESS THAN 1'-6".
4. EMBED SILT SCREEN AT LEAST 1'-0" INTO GROUND AND EXTEND AT LEAST 1'-6" ABOVE GRADE, SUPPORTED ON WOOD FRAME.
5. REMOVE BASIN PROTECTION AFTER TRIBUTARY AREA HAS BEEN PERMANENTLY STABILIZED IN ACCORDANCE WITH THE EPSC PLAN.
6. TRIBUTARY AREA SHALL NOT EXCEED 1 ACRE.

STORMWATER INLET PROTECTION
SCALE: N.T.S.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

No.	Revision	Date	By	Ck	PE	PE #
A	20% ANR Submission	12/5/14	TRC	AMW		
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C	ISSUED FOR USE	3/27/15	TRC	AMW		
D	ISSUED FOR PERMITTING	7/24/15	TRC	AMW		


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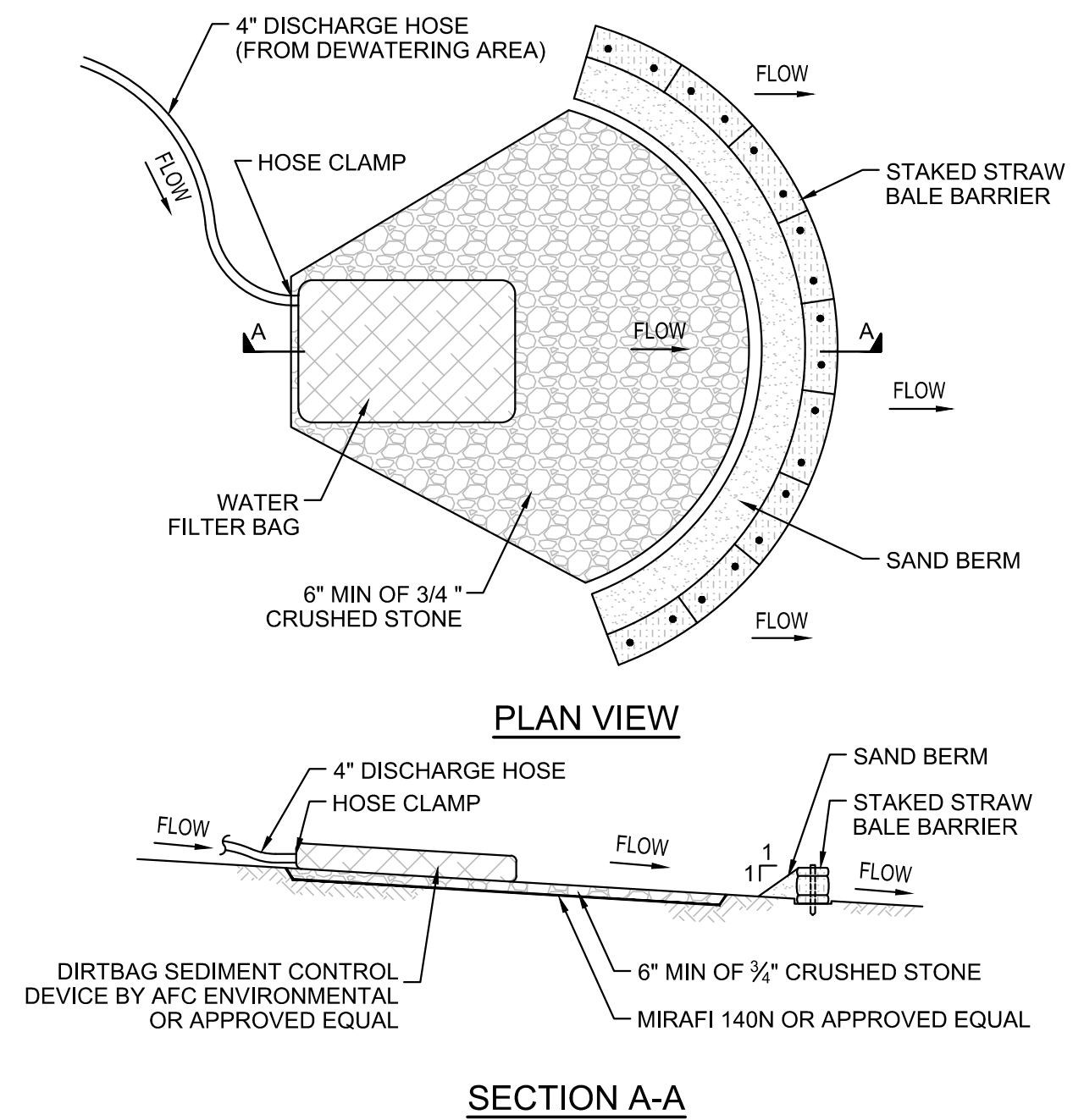


NOTES

- LAY ONE BLOCK ON EACH SIDE OF THE STRUCTURE ON ITS SIDE TO SERVE AS A DEWATERING CHANNEL. FOUNDATION SHALL BE 2 INCHES MINIMUM BELOW THE REST OF THE INLET AND BLOCKS SHALL BE PLACED AGAINST THE INLET FOR SUPPORT.
- CONCRETE BLOCKS SHALL BE PLACED LENGTHWISE IN A SINGLE ROW AROUND THE PERIMETER OF THE INLET. THE ENDS OF EACH BLOCK SHALL BE ABUTTING. THE HEIGHT OF THE BARRIER MAY BE VARIED BY STACKING VARIOUS COMBINATIONS OF DIFFERENT SIZED BLOCKS. THE BARRIER SHALL BE A MINIMUM OF 12 INCHES HIGH AND A MAXIMUM OF 16 INCHES HIGH.
- HARDWARE CLOTH OR 1/2 INCH WIRE MESH SHALL BE PLACED OVER THE OPENINGS OF THE CONCRETE BLOCKS AND EXTENDED AT LEAST 12 INCHES AROUND THE OPENINGS TO PREVENT AGGREGATE FROM BEING TRANSPORTED THROUGH THE OPENINGS IN THE BLOCK.
- USE CLEAN STONE OR GRAVEL 1/2 INCH TO 3/4 INCH IN DIAMETER PLACED 2 INCHES BELOW TOP OF THE BLOCK ON A 2H:1V SLOPE OR FLATTER.
- A 1 FOOT LAYER OF FILTER STONE SHALL BE PLACED AGAINST THE 3 INCH STONE.
- MAXIMUM DRAINAGE AREA PER SEDIMENT TRAP IS 1 ACRE.
- BLOCK AND GRAVEL DROP INLET SEDIMENT FILTER SHALL BE CONSTRUCTED IN PAVED AREAS.

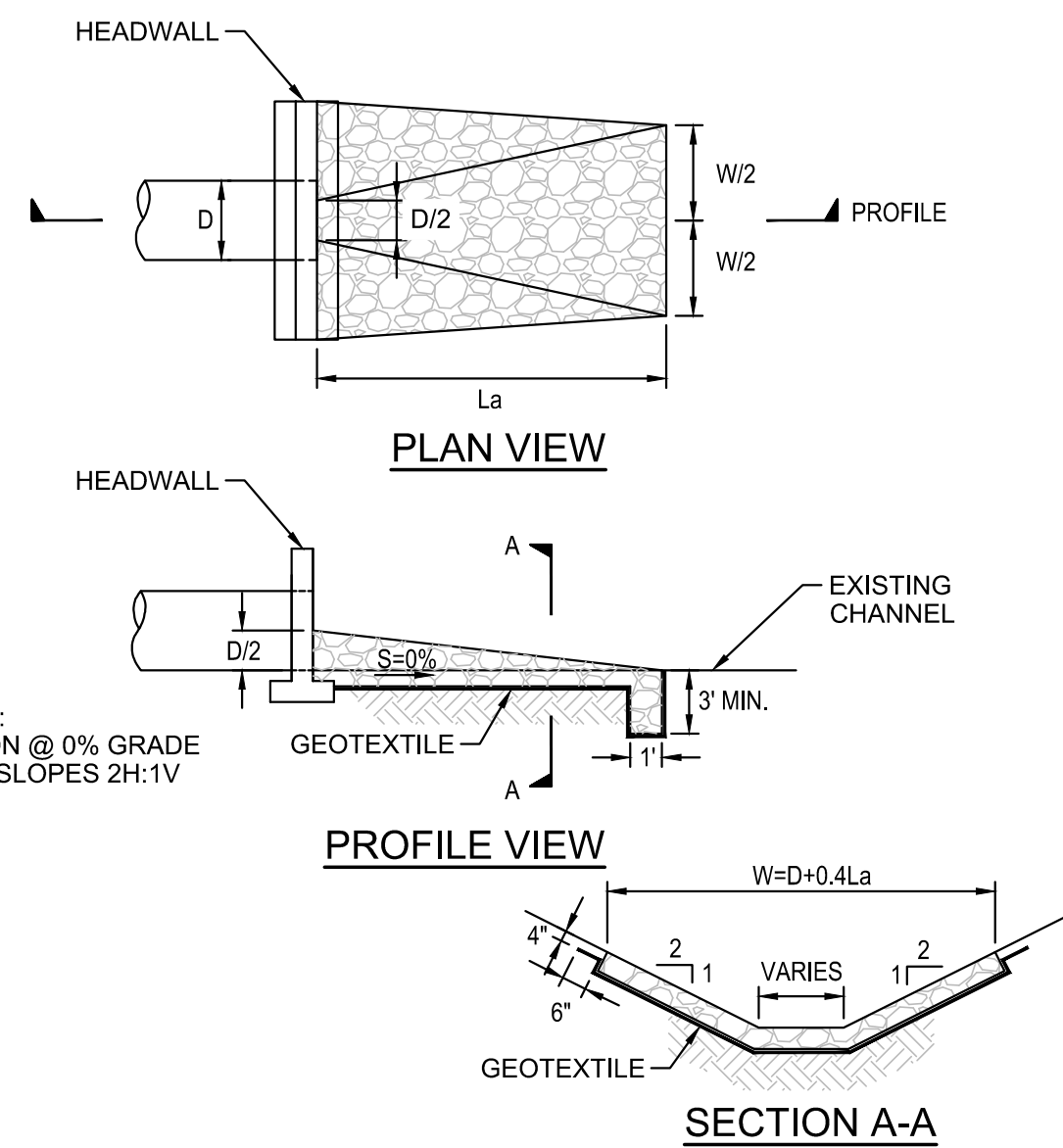
STORMWATER INLET PROTECTION

SCALE: N.T.S.



TYPICAL WATER FILTER BAG

SCALE: N.T.S.

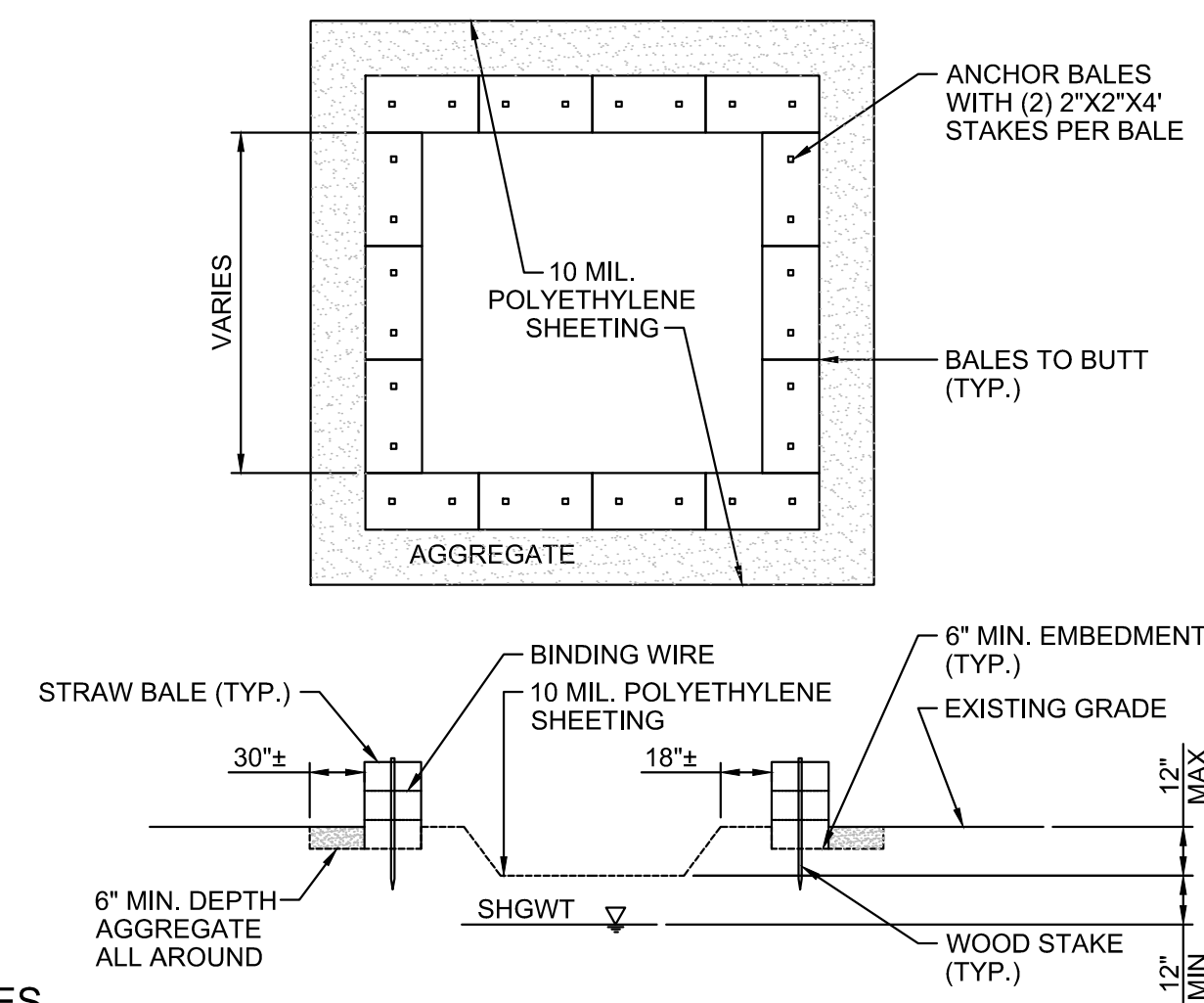


NOTES

- RIPRAP OUTLET PROTECTION DIMENSIONS SHALL BE MODIFIED AS REQUIRED TO ENSURE IT DOES NOT EXTEND OUTSIDE THE ESTABLISHED RIGHT-OF-WAY. MODIFICATION OF OUTLET PROTECTION DESIGN IS SUBJECT TO OWNER AND VTRANS APPROVAL.
- RIP-RAP SHALL BE IN ACCORDANCE WITH VAOT SPECIFICATION 706.03, LIGHT TYPE RIP-RAP. PLACE MATERIAL ON NON-WOVEN GEOTEXTILE BLANKET (MIRAFI 140N OR EQUAL).

RIPRAP OUTLET PROTECTION

SCALE: N.T.S.

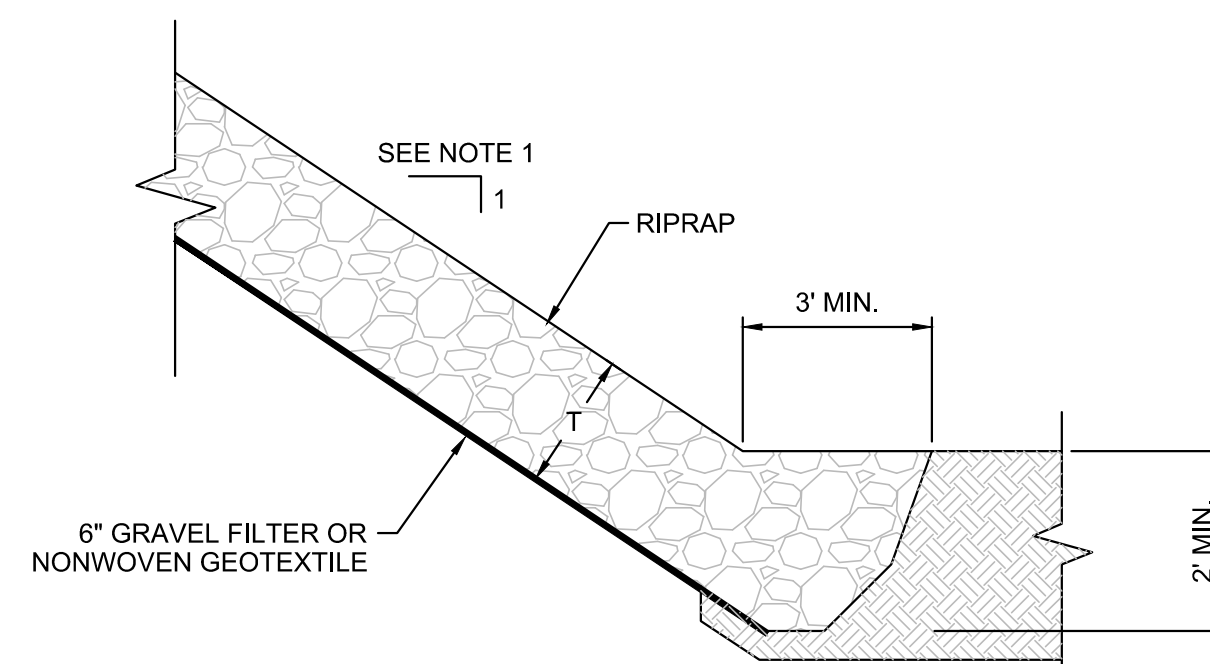


NOTES

- CONTAINMENT SHALL BE STRUCTURALLY SOUND, LEAK FREE AND CONTAIN ALL LIQUID WASTES.
- CONTAINMENT DEVICES SHALL BE OF SUFFICIENT VOLUME TO COMPLETELY CONTAIN THE LIQUID WASTES GENERATED.
- WASHOUT SHALL BE CLEANED OR NEW FACILITIES CONSTRUCTED AND READY TO USE ONCE WASHOUT IS 75% FULL.
- WASHOUT AREA(S) SHALL BE INSTALLED IN A LOCATION EASILY ACCESSIBLE BY CONCRETE TRANSIT-MIX TRUCK AND NO CLOSER THAN 50 FEET FROM RIVERS OR STREAMS.
- ONE OR MORE AREAS MAY BE INSTALLED ON THE CONSTRUCTION SITE AND MAY BE RELOCATED AS CONSTRUCTION PROGRESSES.
- REMOVE ACCUMULATION OF SAND AND AGGREGATE WEEKLY OR MORE FREQUENTLY AND DISPOSE OF PROPERLY.

CONCRETE WASHOUT AREA

SCALE: N.T.S.



NOTES


- ALL SLOPES SUBJECT TO CONCENTRATED RUN-OFF OR CHANNELIZED FLOW STEEPER THAN 3H:1V SHALL BE STABILIZED WITH RIPRAP.
- UNLESS OTHERWISE NOTED, RIPRAP GRADATION SHALL BE $D_{50} = 6"$
- MINIMUM THICKNESS OF RIPRAP COVER SHALL BE THE GREATER OF 15" OR $2.25 \cdot D_{50}$.
- GEOTEXTILE SHALL BE MIRAFI 140NL OR APPROVED EQUAL.
- WHEN APPLIED TO A STREAM BANK, RIPRAP SLOPE PROTECTION SHALL BE GRADED FLUSH WITH UNDISTURBED BANKS UPSTREAM AND DOWNSTREAM FROM THE STABILIZATION SITE AND SHALL NOT ENCRoACH INTO THE WATERWAY.

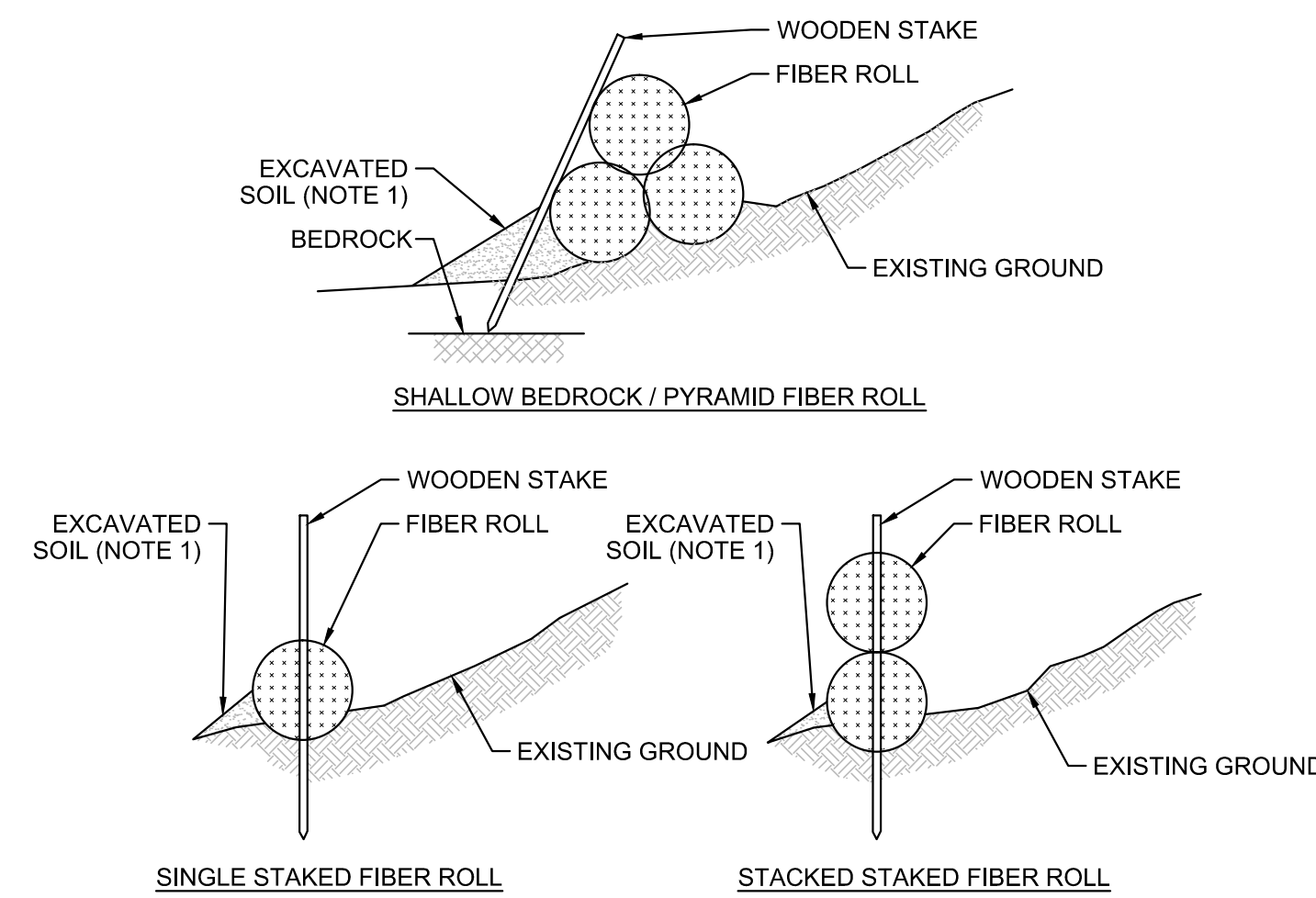
RIPRAP SLOPE PROTECTION

SCALE: N.T.S.

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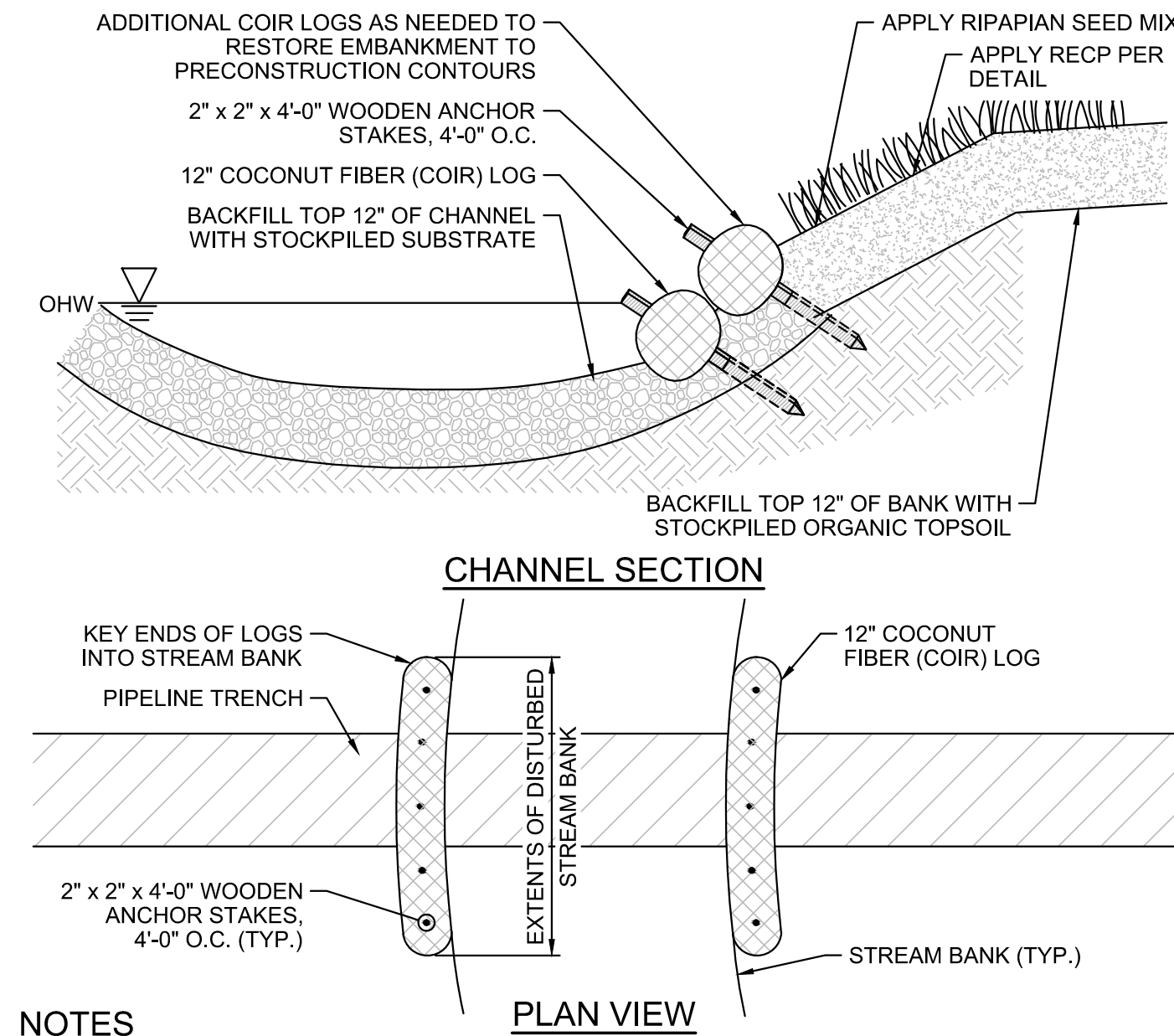
No.	Revision	Date	By	Ck	PE	PE #
A	20% ANR Submission	12/5/14	TRC	AMW		
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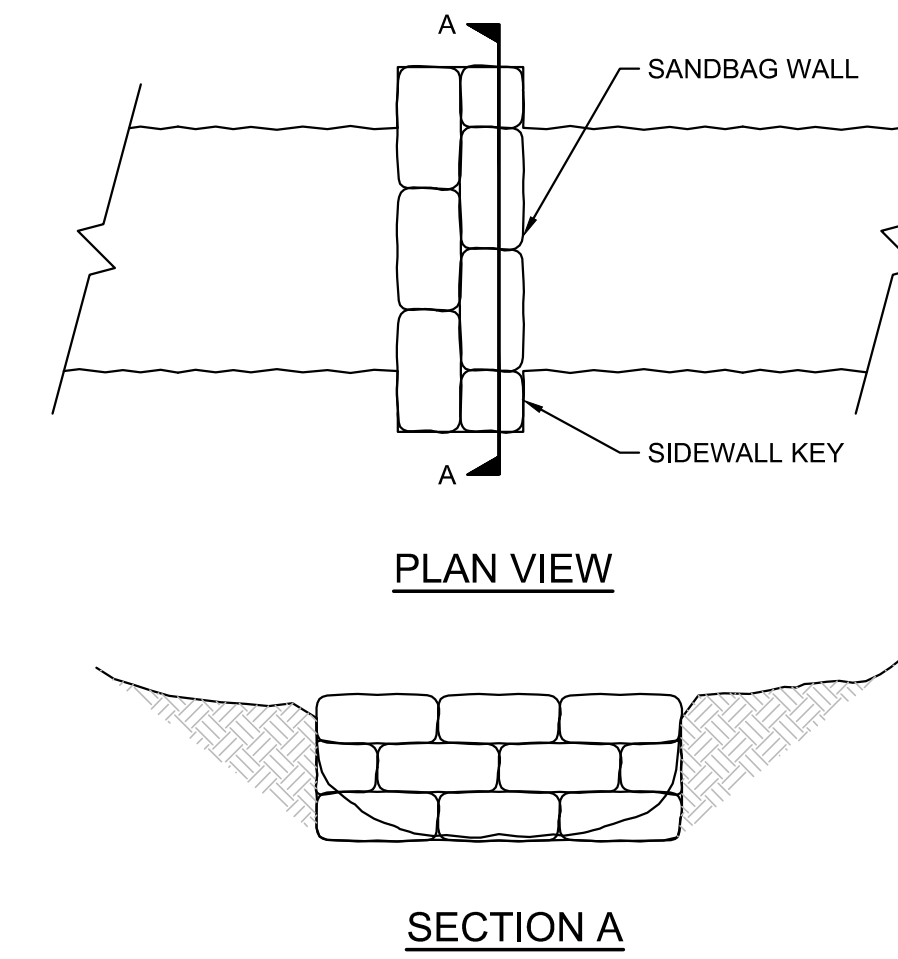
- NOTES**
- FIBER ROLL SHALL BE PLACED IN SHALLOW TRENCH UP TO 4 INCHES, WHERE FEASIBLE, PLACING SOIL REMOVED FROM TRENCH BEHIND THE ROLL.
 - FIBER ROLLS SHALL BE ANCHORED WITH 2 INCH X 2 INCH WOODEN STAKES (36 INCHES LONG), EITHER INSTALLED THROUGH CENTER OF ROLL (AS SHOWN) OR PLACED ON BOTH SIDES OF ROLL.
 - STAKES SHALL BE A MAXIMUM OF 4 FEET ON CENTER.
 - SINGLE OR DOUBLE STACKED STAKED FIBER ROLLS TO BE INSTALLED WHERE SOIL DEPTH ALLOWS, WHERE SHALLOW TO BEDROCK, PYRAMID FIBER ROLLS TO BE UTILIZED WITH STAKES, AS FEASIBLE.
 - FIBER ROLLS TO BE REPLACED OR REPLENISHED AS NEEDED DURING ACTIVE EARTH WORK.
 - PERIMETER CONTROLS SHALL NOT CROSS ACTIVE ROUTES (E.G., ROADS) OR ACTIVE FLOW PATHS (E.G., LARGER STREAMS OR RIVERS).
 - PERIMETER CONTROLS SHALL REMAIN IN PLACE AND BE MAINTAINED/REPLACED AS NEEDED UNTIL FINAL STABILIZATION IN THE AREA HAS BEEN ACHIEVED.

STACKED FIBER ROLL
SCALE: N.T.S.



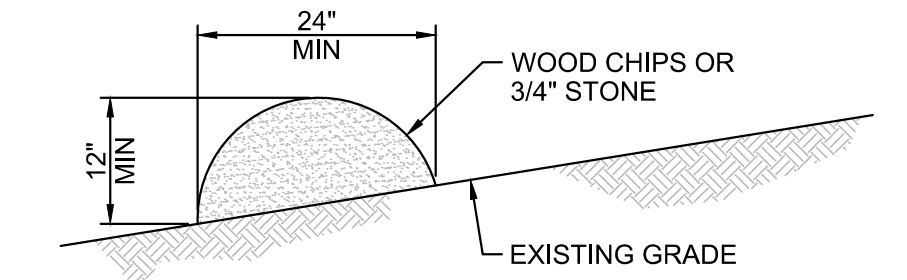
- NOTES**
- APPLY COIR LOG DETAIL TO SITES WHERE STREAM BANK IS DISTURBED OR TRENCHED THROUGH DURING CABLE INSTALLATION AND BANK COMPOSITION PERMITS STAKES TO BE DRIVEN.
 - INSTALL ROLLED EROSION CONTROL PRODUCT (RECP) PRIOR TO INSTALLATION OF COIR LOGS.
 - PLACE COIR LOG IN 2 INCH DEEP TRENCH ALONG SLOPE OF EMBANKMENT AND STAKE INTO PLACE THROUGH RECP.
 - KEY-IN COIR LOG BOTH UPSTREAM AND DOWNSTREAM FROM CABLE TRENCH TO MAKE COIR LOG FLUSH WITH STREAM BANK IN ORDER TO PREVENT UNRAVELING OF BANK DURING HIGH FLOW EVENTS.
 - COIR LOG MESH TO CONSIST OF BIODEGRADABLE MATERIAL.

STREAM BANK RESTORATION WITH COIR LOGS
SCALE: N.T.S.



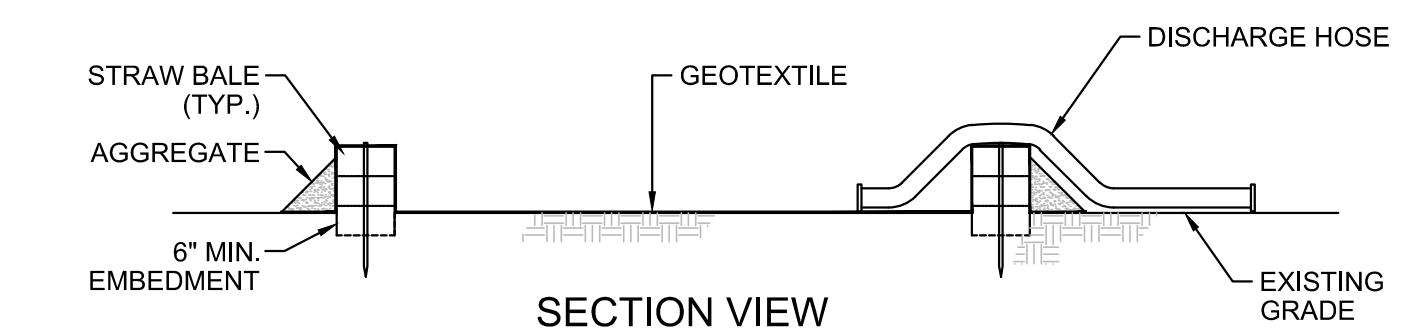
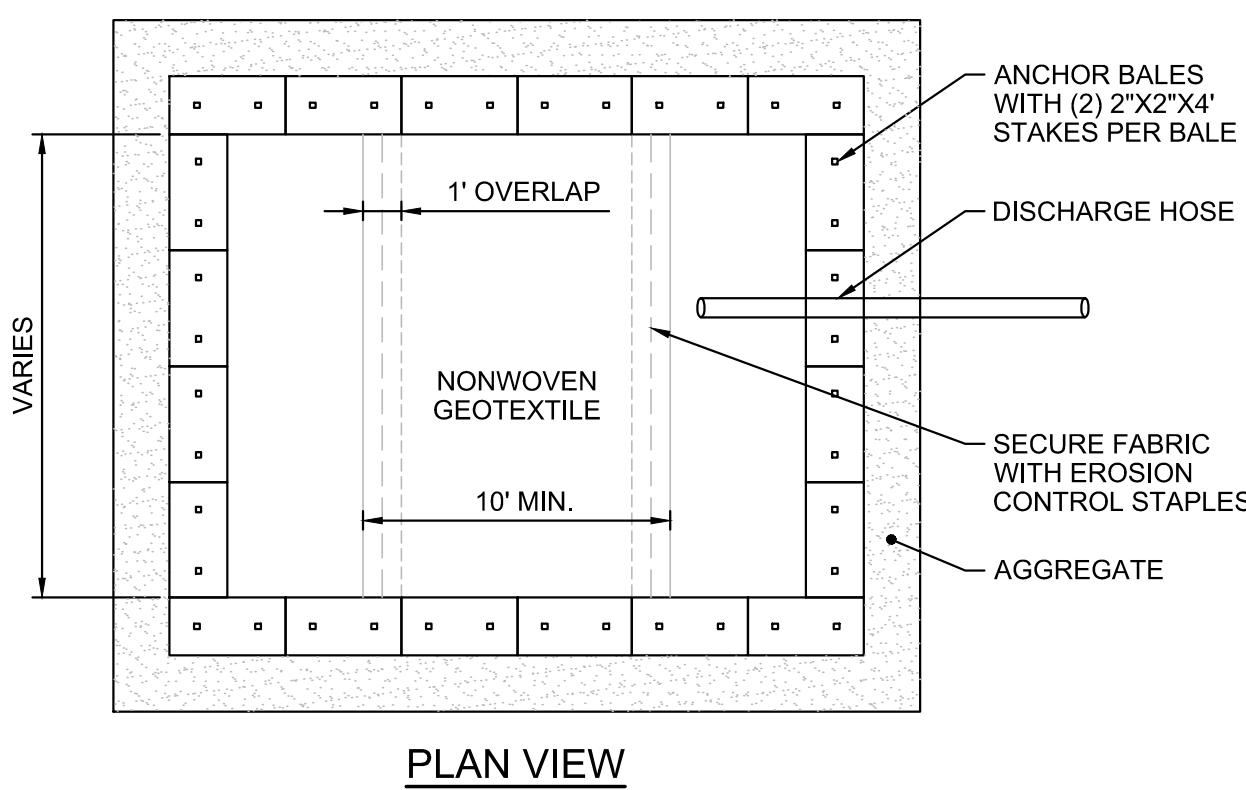
- NOTES**
- AFTER TRENCH EXCAVATION TO EDGE OF STREAM AND WETLAND, HAND DRESS BOTTOM OF TRENCH IN VICINITY OF PLANNED TRENCH BREAKER CONSTRUCTION.
 - EXCAVATE KEY INTO TRENCH SIDEWALL. EXCAVATE TO PROVIDE VERTICAL SURFACE NOT LESS THAN 6" INTO BANK.
 - CONSTRUCT SANDBAG TRENCH BREAKER USING SANDBAGS FILLED WITH CLEAN, FINE SAND. BUILD SANDBAG WALL TO FULL HEIGHT OF NORMAL HIGH WATER.
 - BACKFILL KEY WAY TO PROVIDE COMPACTED NATIVE SOIL AGAINST SANDBAGS.
 - BACKFILL TRENCH CONCURRENT WITH CABLE PLACEMENT. REMOVE TRENCH BREAKER AS CABLE IS PLACED.
 - PROVIDE STREAMBED AND EMBANKMENT PROTECTION PER "VERMONT STANDARDS AND SPECIFICATIONS FOR EROSION PREVENTION AND SEDIMENT CONTROL".

TEMPORARY TRENCH BREAKER
SCALE: N.T.S.



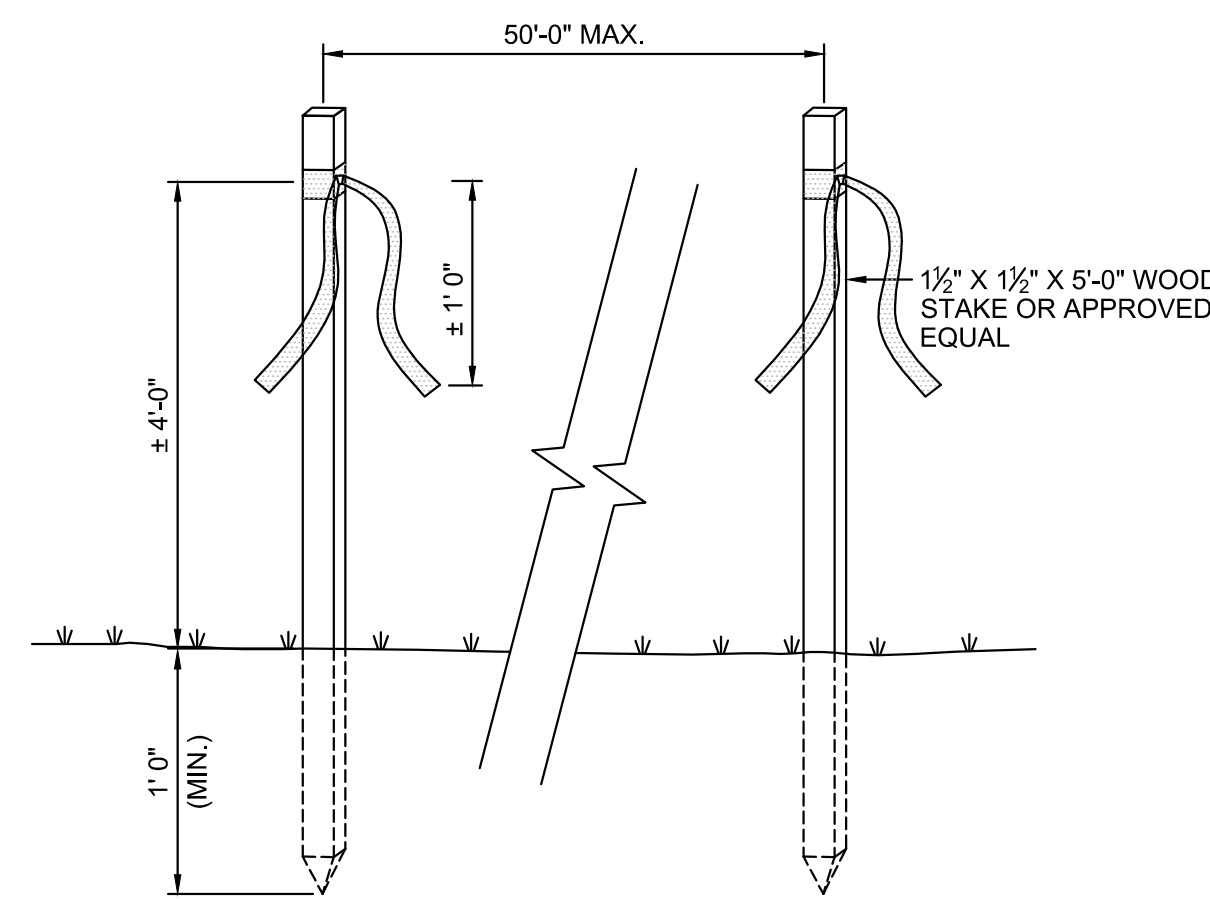
- NOTES**
- BERM SHALL BE COMPRISED OF WOOD CHIPS OR 3/4" STONE.
 - THE BERM SHALL BE PLACED ALONG A RELATIVELY LEVEL CONTOUR.
 - EXISTING GROUND SHALL BE PREPARED AS NEEDED SUCH THAT THE BERM LIES NEARLY FLAT ALONG THE GROUND TO AVOID THE CREATION OF VOIDS AND BRIDGES IN ORDER TO MINIMIZE THE POTENTIAL OF WASH OUTS UNDER THE BERM.
 - ON SLOPES < 5% OR AT THE BOTTOM OF STEEPER SLOPES (<2:1) UP TO 20 FEET LONG, THE BERM SHALL BE MINIMUM OF 12 INCHES HIGH, AS MEASURED ON THE UPHILL SIDE OF THE BERM, AND A MINIMUM OF 2 FEET WIDE. ON LONGER STEEPER SLOPES, THE BERM SHALL BE WIDER TO THE ACCOMMODATE ADDITIONAL FLOW.
 - BERM MAY BE INSTALLED IN PLACE OF SILT FENCE EXCEPT IN, BUT NOT LIMITED TO, THE FOLLOWING AREAS: WETLAND AREAS, AT POINTS OF CONCENTRATED FLOW, BELOW STORMWATER OUTFALLS, AROUND CATCH BASINS AN CLOSED STORM SYSTEMS AND AT THE BOTTOM OF STEEP SLOPES THAT ARE MORE THAN 50 FEET FROM TOP TO BOTTOM.
 - USE OF STONE MAY NOT BE PERMITTED IN CERTAIN AREAS PER LANDOWNER AGREEMENT. WOOD CHIPS MAY BE USED AS AN ALTERNATIVE, THEN SCATTERED ONCE NO LONGER IN USE.
 - PERIMETER CONTROLS SHALL NOT CROSS ACTIVE ACCESS ROUTES (E.G., ROADS) OR ACTIVE FLOW PATHS (E.G., LARGER STREAMS OR RIVERS).
 - PERIMETER CONTROLS SHALL REMAIN IN PLACE AND BE MAINTAINED/REPAIRED AS NEEDED UNTIL FINAL STABILIZATION IN AREA HAS BEEN ACHIEVED.

WOOD CHIP OR STONE BERM
SCALE: N.T.S.



- NOTES**
- GEOTEXTILE SHALL BE MIRAFI 140N OR APPROVED EQUIVALENT.
 - DEWATERING BASINS SHALL BE OF SUFFICIENT VOLUME TO COMPLETELY CONTAIN THE EFFLUENT VOLUME GENERATED. DIMENSIONS MAY VARY.
 - BASINS SHALL BE CLEANED OR NEW FACILITIES CONSTRUCTED AND READY TO USE ONCE BASIN IS 75% FULL. DISPOSE OF SEDIMENT PROPERLY.
 - SEDIMENT BASINS SHALL BE LOCATED NO CLOSER THAN 50 FEET FROM RIVERS, STREAMS, OR OTHER SENSITIVE NATURAL RESOURCES.
 - ONE OR MORE BASINS MAY BE INSTALLED ON THE CONSTRUCTION SITE AND MAY BE RELOCATED AS CONSTRUCTION PROGRESSES.

DEWATERING BASIN
SCALE: N.T.S.




- NOTES**
- ORANGE DEMARCATION FLAGGING SHALL BE LOCATED AT INTERVALS OF NO GREATER THAN 50'-0" ON CENTER.
 - FLAGGING MAY BE FASTENED TO TREE TRUNKS, 1 1/2" x 1 1/2" WOOD STAKES, OR METAL CONSTRUCTION FENCE POSTS.
 - FLAGGING SHALL BE APPROXIMATELY FOUR FEET ABOVE GROUND. TIED ENDS OF FLAGGING SHALL BE LEFT WITH APPROXIMATELY 1-FOOT LONG TAILS.

FLAGGING DETAIL
SCALE: N.T.S.

Designed	TRC
Drawn	TRC
Checked	-
Approved	-
Scale	AS NOTED

No.	Revision	Date	By	Ck	PE	PE #
A	20% ANR Submission	12/5/14	TRC	AMW		
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